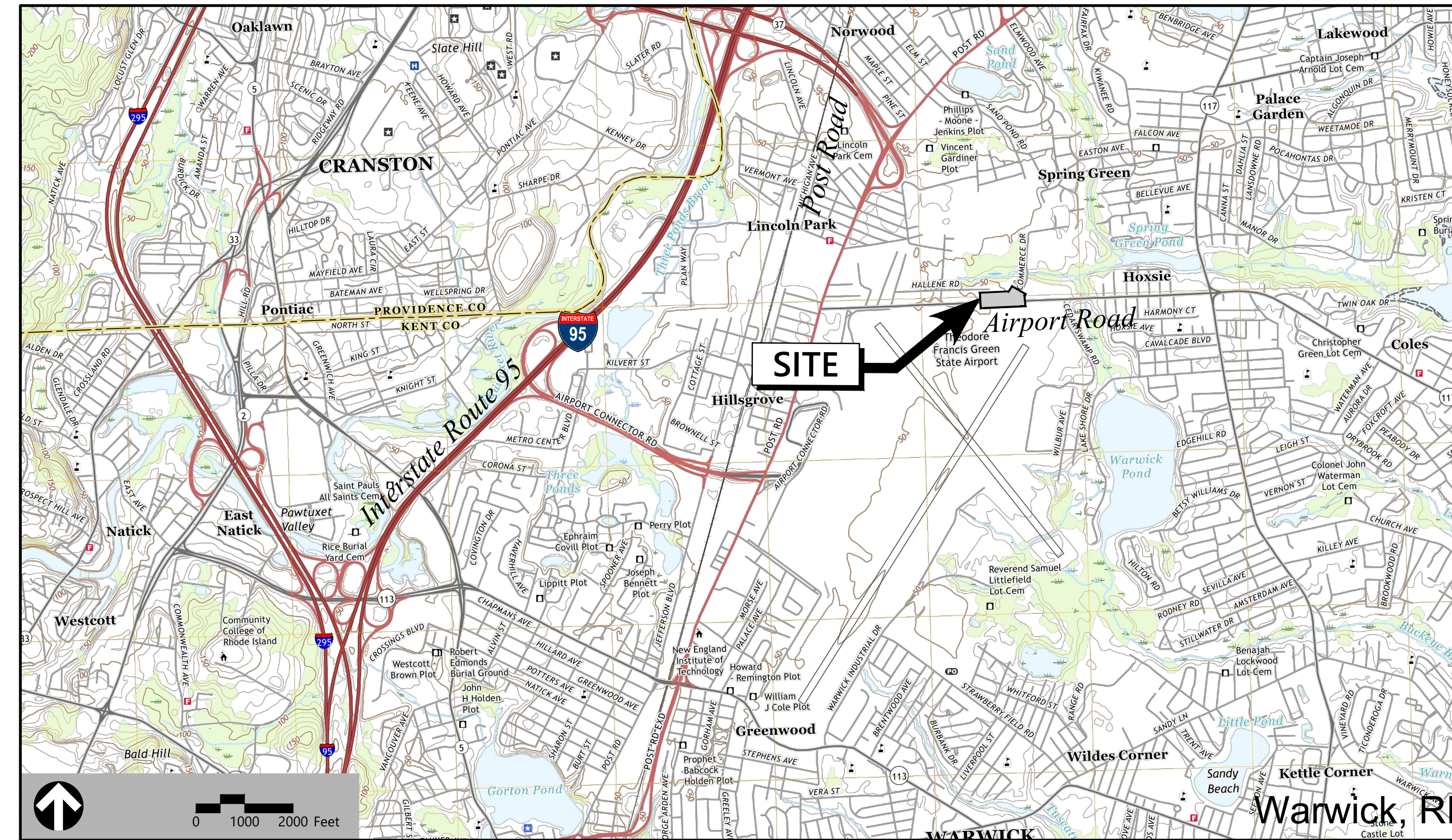


Roadway Plans

| | |
|--------------|---------------------|
| Issued for | Final Plan Approval |
| Date Issued | January 28, 2022 |
| Latest Issue | February 22, 2022 |

Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island



Owner

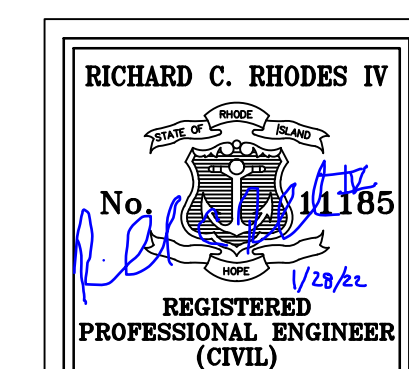
NP Airport Road Warwick, LLC
4825 NW 41st Street, Suite 500
Riverside, MO 64150

Applicant

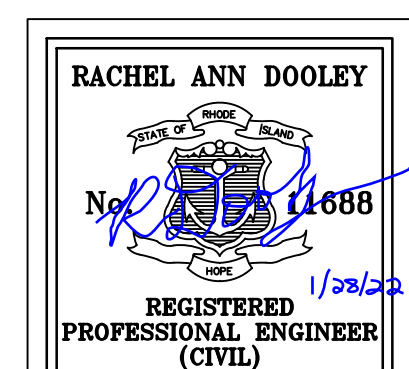
NorthPoint Development, LLC
4805 Montgomery Road
Suite 310
Cincinnati, OH 45212

Sheet Index

| No. | Drawing Title | Latest Issue |
|------|---|--------------|
| C-1 | Cover | 02/22/2022 |
| C-2 | Standard Plan Symbols & Standard Legend - 1 | 02/22/2022 |
| C-3 | Standard Plan Symbols & Standard Legend - 2 | 02/22/2022 |
| C-4 | Standard Notes - 1 | 02/22/2022 |
| C-5 | Standard Notes - 2 | 02/22/2022 |
| C-6 | Job Specific Plan Symbols, Legend & Notes | 02/22/2022 |
| C-7 | Typical Sections | 02/22/2022 |
| C-8 | General Plan | 02/22/2022 |
| C-9 | Location Plan | 02/22/2022 |
| C-10 | Signing & Striping Plan | 02/22/2022 |
| C-11 | Traffic Signal Plan | 02/22/2022 |
| C-12 | Traffic Control Plan 01 | 02/22/2022 |
| C-13 | Traffic Control Plan 02 | 02/22/2022 |
| C-14 | Cross Sections | 02/22/2022 |



Sheets C-1 to C-9, C-14



Sheets C-10 to C-13



1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100

Proposed Airport Rd/Commerce Drive Intersection Improvements

Warwick, Rhode Island

| No. | Revision | Date | Apprv. |
|-----|---------------------|-----------|--------|
| 1 | Final Plan Approval | 2/22/2022 | |

| Designed by | Checked by |
|-------------|------------|
| Issued for | Date |

Construction January 28, 2022

Standard Plan Symbols & Standard Legend - 1

Drawing Number

C-2

EXISTING

EDGE OF PAVEMENT

BERM

CURB

GUARDRAIL

MAILBOX

UTILITY POLE

POLE GUY

LUMINARE

SIGN

SUBDRAIN

STORMDRAIN

SANITARY SEWER

WATER MAIN

GAS MAIN

TELEPHONE DUCT

ELECTRIC DUCT

PLUG AND CAP PIPE

ABANDONED UTILITY

FLARED END SECTION

HEADWALL

WATER OR GAS GATE

CATCH BASIN

MANHOLE

HYDRANT

BASELINE OR CENTERLINE

STATE HIGHWAY LINE

STATE FREEWAY LINE

PERMANENT EASEMENT LINE

TEMPORARY EASEMENT LINE

PROPERTY LINE

CITY OR TOWN LINE

PAVED WATERWAY

CONTOUR LINE

OPEN DITCH

R.I. HIGHWAY BOUND

STONE BOUND

RETAINING WALL

FIELD STONE WALL

BORINGS

FENCE

WOOD OR BRUSH LINE

TREES

RIVER OR STREAM

WETLAND AREA

BUILDING

FOUNDATION

BUILDING TO BE REMOVED

RAILROAD TRACKS

CUT AND MATCH

RIP-RAP

CUT SLOPE

FILL SLOPE

ROCK CUT

SPOT GRADE

AREA GRADED TO DRAIN

BALED HAY RI STD 9.1.0

BALED HAY & SILT FENCE RI STD. 9.3.0

EDGE OF WETLAND

WETLAND PERIMETER

AREA SUBJECT TO STORM FLOW

100-YEAR FLOOD PLAIN

LIMIT OF DISTURBANCE

LIMIT OF CLEARING

NEW

N(SIZE)SD

(Length Size)

N(SIZE)W

N(SIZE)G

N-#(SIZE)T

N-#(SIZE)E

NEW S.H.L. PLAT NO. XX

NEW S.F.L. PLAT NO. XX

NEW P.E.B.

NEW T.E.B.

ELEV

NO.

ELEV. X

LIMIT OF DISTURBANCE

LIMIT OF CLEARING

- (1.1.0)
- (1.3.0)
- (2.1.0)
- (2.2.0)
- (2.3.0) (DIA.)
- (3.2.0)
- (3.2.1) (DIA.)
- (3.3.0)
- (3.3.2)
- (3.3.3)
- (3.4.0)
- (3.4.1)
- (3.4.2)
- (3.4.3)
- (3.4.4)
- (3.4.5) (DIA.)
- (3.5.0)
- (3.5.1) (SIZE)
- (3.6.0)
- (3.7.0) (DIA.)
- (4.2.0)
- (4.2.1)
- (4.2.2)
- (4.3.0) (SIZE)
- (4.4.0) (DIA.)
- (4.5.0)
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- (4.5.2)
- (5.3.0)
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- (6.2.0)
- (6.2.1)
- (6.3.0)
- (6.3.1)
- (6.3.2)
- (6.3.3)
- (6.3.4)
- (6.4.0)
- (7.1.0S)
- (7.1.0C)
- (7.1.1)
- (7.1.2)
- (7.1.4)
- (7.1.5)
- (7.1.6)
- (7.1.7)
- (7.1.8)
- (7.2.0S)
- (7.2.0C)
- (7.2.1)
- (7.2.2)
- (7.3.0S)
- (7.3.0C)
- (7.3.1)
- (7.3.2)
- (7.3.3)
- (7.3.4)
- (7.3.5)
- (7.3.6)
- (7.3.7)
- (7.3.8)
- (7.4.0)
- (7.4.1)

- UNDERDRAIN
- CONCRETE CONNECTING COLLAR
- CONCRETE HEADWALLS FOR PIPE CULVERTS
- STANDARD HEADWALLS FOR MULTIPLE 3'-6" TO 7'-0" PIPE CULVERTS
- PRECAST CONCRETE FLARED END SECTION
- BRICK/SOLID BLOCK 4'-0" ROUND MANHOLE
- BRICK/SOLID BLOCK 5'-0" OR 6'-0" ROUND MANHOLE
- BRICK/SOLID BLOCK TYPE "D" SQUARE CATCH BASIN
- BRICK/SOLID BLOCK TYPE "F" SQUARE CATCH BASIN
- SOLID BLOCK FLUSH SQUARE CATCH BASIN
- BRICK/SOLID BLOCK TYPE "D" ROUND CATCH BASIN
- BRICK/SOLID BLOCK ROUND CATCH BASIN WITH GUTTER INLET
- BRICK/SOLID BLOCK TYPE "F" ROUND CATCH BASIN
- BRICK/SOLID BLOCK TYPE "R" CATCH BASIN
- SOLID BLOCK FLUSH ROUND CATCH BASIN
- BRICK/SOLID BLOCK 5'-0" OR 6'-0" ROUND CATCH BASIN
- SOLID BLOCK SHALLOW TYPE "F" SQUARE CATCH BASIN
- SOLID BLOCK SHALLOW 5'-0" OR 6'-0" SQUARE CATCH BASIN
- BRICK/SOLID BLOCK DROP INLET
- BRICK/SOLID BLOCK ROUND MANHOLE OR CATCH BASIN GREATER THAN 12'-0"
- PRECAST 4'-0" ROUND MANHOLE
- PRECAST 5'-0" ROUND MANHOLE
- PRECAST 6'-0" ROUND MANHOLE
- PRECAST 4'-0" OR 6'-0" SQUARE MANHOLE OR CATCH BASIN
- PRECAST 4'-0", 5'-0", OR 6'-0" ROUND CATCH BASIN
- PRECAST CONCRETE DROP INLET
- PRECAST CONCRETE DROP INLET LATERAL OUTLET
- PRECAST CONCRETE DROP INLET LONGITUDINAL OUTLET
- CATCH BASIN AND MANHOLE STEP
- CONCRETE COLLARS
- LIGHT-DUTY SQUARE FRAME AND ROUND COVER
- HEAVY DUTY SQUARE FRAME AND ROUND COVER
- LIGHT-DUTY ROUND FRAME AND COVER
- HEAVY-DUTY ROUND FRAME AND COVER
- SQUARE FRAME AND GRATE
- SQUARE FRAME AND GRATE
- SQUARE FRAME AND GRATE (BICYCLE SAFE)
- HIGH CAPACITY FRAME AND GRATE
- HIGH CAPACITY FRAME AND GRATE (BICYCLE SAFE)
- ROUND FRAME AND GRATE
- PRECAST CONCRETE CURB (STRAIGHT)
- PRECAST CONCRETE CURB (CIRCULAR)
- 3'-0" PRECAST CONCRETE TRANSITION CURB
- 6'-0" PRECAST CONCRETE TRANSITION CURB
- PRECAST 2'-0" RADIUS CORNER
- PRECAST CONCRETE INLET STONE (FOR SQUARE CATCH BASIN)
- PRECAST CONCRETE INLET STONE (FOR ROUND CATCH BASIN)
- PRECAST CONCRETE APRON STONE (FOR SQUARE CATCH BASIN)
- PRECAST CONCRETE APRON STONE (FOR ROUND CATCH BASIN)
- PRECAST CONCRETE SLOPED FACE CURB (STRAIGHT)
- PRECAST CONCRETE SLOPED FACE CURB (CIRCULAR)
- PRECAST CONCRETE SLOPED FACE TRANSITION CURB
- PRECAST CONCRETE TRANSITION CURB (VERTICAL FACE TO SPLOPED FACE)
- GRANITE CURB (STRAIGHT)
- GRANITE CURB (CIRCULAR)
- 3'-0" GRANITE TRANSITION CURB
- 6'-0" GRANITE TRANSITION CURB
- GRANITE WHEELCHAIR RAMP TRANSITION CURB
- GRANITE 2'-0" RADIUS CORNER
- GRANITE INLET STONE (FOR SQUARE CATCH BASIN)
- GRANITE INLET STONE (FOR ROUND CATCH BASIN)
- GRANITE APRON STONE (FOR SQUARE CATCH BASIN)
- GRANITE APRON STONE (FOR ROUND CATCH BASIN)
- GRANITE SLOPED FACE CURB
- GRANITE SLOPED FACE TRANSITION CURB

- (7.4.2)
- (7.5.0)
- (7.5.1A)
- (7.5.1B)
- (7.6.0)
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- (8.3.0)
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- (51.2.0)
- (51.3.0)
- (51.4.0)

- GRANITE TRANSITION CURB (VERTICAL FACE TO SLOPE FACE)
- BITUMINOUS CONCRETE LIP CURB
- BITUMINOUS BERM (CONSTRUCTION METHOD A)
- BITUMINOUS BERM (CONSTRUCTION METHOD B)
- CURB SETTING DETAIL
- BITUMINOUS CONCRETE DITCH
- RIP-RAP DITCH
- PAVED WATERWAY
- BALED HAY EROSION CHECK
- SILT FENCE DETAIL
- BALED HAY DITCH EROSION CHECK AND SILT FENCE COMBINED
- BALED HAY DITCH AND SWALE EROSION CHECK
- LOG AND HAY CHECK DAM
- DEWATERING BASIN
- BALED HAY CATCH BASIN INLET PROTECTION
- CONSTRUCTION ACCESS
- WET STONE MASONRY RETAINING WALL
- RUBBLE MASONRY WALL
- CONCRETE RETAINING WALL
- STONE MASONRY STEPS
- CONCRETE HIGHWAY BOUND
- POST AND MOUNTINGS FOR RURAL MAILBOX
- POST AND MULTIPLE MOUNTINGS FOR RURAL MAILBOXES
- PRECAST TYPE "A" HANDHOLE
- HEAVY DUTY TYPE "H" HANDHOLE
- ALUMINUM LIGHTING STANDARDS
- BI-DIRECTIONAL CONTROL DEVICE
- STREET SIGN MOUNTING DETAIL
- POLYETHYLENE DRUM WITH MARKINGS
- PVC PLASTIC PIPE TYPE III BARRICADE
- CHAIN LINK FENCE 3'-0" TO 4'-0"
- CHAIN LINK FENCE 5'-0" TO 6'-0"
- CHAIN LINK FENCE 5'-0" TO 6'-0" INTERMEDIATE POST
- WOVEN WIRE RIGHT-OF-WAY FENCE (STEEL POST)
- TYPICAL GUARDRAIL INSTALLATION
- STEEL BEAM GUARDRAIL
- STEEL BEAM GUARDRAIL DETAILS
- STEEL BEAM GUARDRAIL DOUBLE FACED ASSEMBLY
- STEEL BEAM GUARDRAIL FIXTURES
- STEEL BEAM GUARDRAIL REFLECTORIZED TRIANGULAR DELINEATOR
- GUARDRAIL END SECTION
- TERMINAL END SECTION (SINGLE FACE)
- ANCHORAGE DETAILS APPROACH END SECTION
- ANCHORAGE DETAILS TRAILING END SECTION
- STEEL BACKED TIMBER GUARDRAIL
- STEEL BACKED TIMBER GUARDRAIL TERMINAL SECTION-TYPE 1
- DOUBLE-FACED PRECAST MEDIAN BARRIER
- SINGLE-FACED PRECAST MEDIAN BARRIER
- SINGLE-FACED PRECAST MEDIAN BARRIER
- PRECAST MEDIAN BARRIER TRANSITION UNIT
- PRECAST MEDIAN BARRIER FOR TEMPORARY TRAFFIC CONTROL
- CEMENT CONCRETE SIDEWALK
- BITUMINOUS CONCRETE SIDEWALK
- WHEELCHAIR RAMP
- WHEELCHAIR RAMP FOR LIMITED RIGHT-OF-WAY AREAS
- DRIVEWAY DEVELOPMENT FOR 3'-0" TRANSITION CURB
- DRIVEWAY DEVELOPMENT FOR 6'-0" TRANSITION CURB
- CEMENT CONCRETE DRIVEWAYS
- DETECTABLE WARNING SYSTEM
- TREE PROTECTION DEVICE
- DRIP LINE TREE PROTECTION DEVICE FOR EXISTING TREES
- SHRUB PROTECTION DEVICE
- TREE WELL
- TREE WALL

- (AB)
- (ABM)
- (AC)
- (AD)
- (AE)
- (AFC)
- (AFG)
- (AG)
- (AHH)
- (AS)
- (AT)
- (AW)
- (BCD)
- (BPS)
- (CCB)
- (CCP)
- (CFP)
- (CG)
- (CMH)
- (CP) (DEPTH)
- (CPP)
- (DB)
- (DC)
- (DCB)
- (DDI)
- (DF)
- (DFC)
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- (DG)
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- (DL)
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- (DMM)
- (DOW)
- (DP)
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- (DRB)
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- (DSS)
- (DSW)
- (DTD)
- (DUP)
- (DWW)
- (FF)
- (GET)
- (IA)
- (IDL)
- (LOD)
- (LOR)
- (LS)

- ADJUST CATCH BASIN TO GRADE
- ADJUST CATCH BASIN TO MANHOLE
- ADJUST CURB STOP TO GRADE
- ADJUST DRAINAGE MANHOLE TO GRADE
- ADJUST ELECTRIC MANHOLE TO GRADE
- ADJUST FRAME AND COVER TO GRADE
- ADJUST FRAME AND GRATE TO GRADE
- ADJUST GAS GATE BOX TO GRADE
- ADJUST HANDHOLE TO GRADE
- ADJUST SANITARY SEWER MANHOLE TO GRADE
- ADJUST TELEPHONE MANHOLE TO GRADE
- ADJUST WATER GATE BOX TO GRADE
- BITUMINOUS CONCRETE DRIVEWAY
- 3" BITUMINOUS CONCRETE TYPE 1-2
- 8" GRAVEL BORROW SUBBASE COURSE
- BUILD NEW STRUCTURE OVER EXISTING PIPE
- CLEAN CATCH BASIN
- CUT AND CAP PIPE WITH RESTRAINT (ALL SIZES)
- CLEAN AND FLUSH PIPE
- CLEARING AND GRUBBING
- CLEAN MANHOLE
- (DEPTH) COLD PLANE
- CUT AND PLUG PIPE (ALL TYPES, ALL SIZES)
- REMOVE AND DISPOSE BITUMINOUS CURB
- REMOVE AND DISPOSE CONCRETE CURB
- REMOVE AND DISPOSE CATCH BASIN
- REMOVE AND DISPOSE DROP INLET
- REMOVE AND DISPOSE FENCE
- REMOVE AND DISPOSE FRAME AND COVER
- REMOVE AND DISPOSE FLARED END SECTION
- REMOVE AND DISPOSE FRAME AND GRATE
- REMOVE AND DISPOSE FIRE HYDRANT
- REMOVE AND DISPOSE FLEXIBLE PAVEMENT
- REMOVE AND DISPOSE GUARDRAIL
- REMOVE AND DISPOSE HEADWALL
- REMOVE AND DISPOSE HIGHWAY BOUND
- REMOVE AND DISPOSE HANDHOLE
- REMOVE AND DISPOSE LIGHT AND FOUNDATION
- REMOVE AND DISPOSE MEDIAN BARRIER
- REMOVE AND DISPOSE MANHOLE
- REMOVE AND DISPOSE MEDIAN MARKER
- REMOVE AND DISPOSE OBSERVATION WELL
- REMOVE AND DISPOSE PIPE
- REMOVE AND DISPOSE PAVEMENT AND RIGID BASE
- REMOVE AND DISPOSE RIGID BASE
- REMOVE AND DISPOSE SIGN
- REMOVE AND DISPOSE TRAFFIC SIGNAL SYSTEM
- REMOVE AND DISPOSE SIDEWALK
- REMOVE AND DISPOSE TELEPHONE DUCT BANKS
- REMOVE AND DISPOSE UTILITY POLE
- REMOVE AND DISPOSE PAVED WATERWAY
- FILTER FABRIC RIPRAP FLARED END UNDERLAYMENT
- FLARED GUARDRAIL END TREATMENT
- IMPACT ATTENUATOR
- IMPERVIOUS DITCH LINER
- LIMIT OF DISTURBANCE
- LIMIT OF REGRADING
- 4" LOAM AND SEED

| | |
|---------------|---|
| NFH | NEW FIRE HYDRANT WITH GATE VALVE |
| NIC | NOT IN THIS CONSTRUCTION CONTRACT |
| NWB | FURNISH AND INSTALL NEW WATER GATE VALVE BOX |
| NWVB | FURNISH AND INSTALL NEW WATER GATE VALVE AND BOX |
| NWCB | FURNISH AND INSTALL NEW WATER CURB STOP BOX |
| NWSB | FURNISH AND INSTALL NEW WATER CURB STOP AND BOX |
| PCD | PERMANENT CHECK DAM |
| PS | 4" PLANTABLE SOIL AND SEED |
| RCB | RECONSTRUCT TYPE "D" CATCH BASIN, TO CATCH BASIN WITH GUTTER INLET |
| RCM | R.I.D.O.T. COMMUNICATIONS MANHOLE |
| RHH | REMOVE, HANDLE, HAUL, TRIM, RESET CURB EDGING, STRAIGHT, CIRCULAR (ALL TYPES) |
| RLP | RELOCATE LAMP POST |
| RMB | RELOCATE MAILBOX (BY OTHERS) |
| RPM | REMOVE PAVEMENT MARKINGS |
| RRP | RIP--RAP PAD (SEE DETAIL) |
| RRS | REMOVE AND RELOCATE SIGN |
| RUP | RELOCATE UTILITY POLE (BY OTHERS) |
| SB | STONE BAFFLE |
| SBAE | STEEL BEAM BRIDGE CONNECTION APPROACH END (W/O NESTED RAIL) |
| SBTE | STEEL BEAM BRIDGE CONNECTION TRAILING END (W/NESTED RAIL) |
| SD- | STRUCTURAL DISPOSITION - SEE CS PAGES OF SPECIFICATION |
| SF | REMOVE AND STOCKPILE FENCE |
| SGA | SPECIAL GRADED AGGREGATE |
| SGC | REMOVE AND STOCKPILE GRANITE CURB |
| SGR | REMOVE AND STOCKPILE GUARDRAIL |
| SH | REMOVE AND STOCKPILE HYDRANT |
| SS | REMOVE AND STOCKPILE SIGN |
| STS | REMOVE AND STOCKPILE TRAFFIC SIGNAL SYSTEM |
| TB | CONCRETE THRUST BLOCK |
| TEP | TIE EXISTING PIPE INTO NEW STRUCTURE |
| TNP | TIE NEW PIPE INTO EXISTING STRUCTURE |
| TBT | THRIE BEAM TRANSITION |
| TBBC | THRIE BEAM BRIDGE CONNECTION |
| TT | TREE TRIMMING |
| WCM | 4" WOOD CHIP MULCH |
| 4DY | 4" EPOXY RESIN PAVEMENT MARKINGS - DOUBLE YELLOW |
| 6W | 6" EPOXY RESIN PAVEMENT MARKINGS - WHITE |
| 12W | 12" EPOXY RESIN PAVEMENT MARKINGS - WHITE |
| 6WT | 6" PREFORMED PATTERNED MARKING (HIGH PERFORMANCE TAPE) |
| 4Y | 4" EPOXY RESIN PAVEMENT MARKINGS - YELLOW |
| 6Y | 6" EPOXY RESIN PAVEMENT MARKINGS - YELLOW |
| P.G.L. | PROFILE GRADE LINE |



1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100

Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island

| No. | Revision | Date | Apprv. |
|-----|---------------------|-----------|--------|
| 1 | Final Plan Approval | 2/22/2022 | |

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| Designed by | Checked by |
|-------------|------------|

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| Issued for | Date |
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Construction

January 28, 2022

Standard Plan Symbols & Standard Legend - 2

Drawing Number

C-3

Sheet 3 of 14

Project Number
73180.00



1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100

GENERAL NOTES:

1. ANY DAMAGE TO EXISTING PAVEMENT, BRIDGES, CONDUIT, SIDEWALK, FENCES, ETC., CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE STATE.
2. THE CONTRACTOR SHALL PLACE ALL EQUIPMENT AND MATERIAL AS FAR AWAY AS POSSIBLE FROM THE EDGE OF THE TRAVEL LANE SO AS NOT TO CAUSE A SAFETY HAZARD, IN ACCORDANCE WITH SECTION 106.06 OF THE R.I.D.O.T. STANDARD SPECIFICATION, LATEST EDITION.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT THE EXISTING CONDITIONS ARE NOT OBLITERATED BEFORE CONTROL POINTS ARE LOCATED AND CONSTRUCTION LAYOUT IS ESTABLISHED. THE CONSTRUCTION LAYOUT SHALL BE PROVIDED IN SUFFICIENT DETAIL, THEREBY ENABLING HIM TO CONSTRUCT THE PROJECT IN CONFORMITY WITH THE PLANS AND SPECIFICATIONS. SURVEY WILL BE PROVIDED BY THE CONTRACTOR. THE RESIDENT ENGINEER WILL NOT AUTHORIZE CONSTRUCTION ACTIVITIES TO BEGIN UNTIL HE IS SATISFIED THAT ALL GROUND CONTROL HAS BEEN ESTABLISHED, TIED DOWN, AND DULY RECORDED IN STANDARD FIELD BOOKS.
4. ALL R.I. STD. 9.9.0 CONSTRUCTION ACCESS ROADS SHALL BE CONSTRUCTED PRIOR TO ANY ROADWAY ACCEPTING CONSTRUCTION TRAFFIC.
5. THE FREQUENCY AND APPLICATION RATES FOR THE DUST CONTROL ITEMS WILL BE AS DIRECTED BY THE ENGINEER.
6. ALL SIDEWALK AND DRIVEWAYS DESIGNATED FOR REPLACEMENT SHALL BE CUT AND MATCHED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
7. ASPHALT EMULSION TACK COAT SHALL BE PLACED PRIOR TO PAVEMENT PLACEMENT ON THE CONCRETE BASE OR COLD PLANED PAVEMENT, AND ON ANY NEW COURSE WHICH HAS BEEN OPEN TO TRAFFIC, OR ANY NEW COURSE WHICH HAS BEEN EXPOSED FOR MORE THAN 3 DAYS, AND/OR AS DIRECTED BY THE ENGINEER. IT SHALL ALSO BE APPLIED TO VERTICAL PAVEMENT FACES BETWEEN ADJOINING PAVEMENT SECTIONS. ALL APPLICATIONS ON BOTH HORIZONTAL AND VERTICAL SURFACES SHALL BE PAID FOR UNDER THE CONTRACT UNIT BID PRICE FOR CODE 403.0300 "ASPHALT EMULSION TACK COAT."
8. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS. IN ADDITION TO THOSE AREAS SPECIFICALLY DESIGNATED ON THE PLANS, THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND PLACING, AT HIS OWN EXPENSE, PLANTABLE SOIL AND SEED IN AREAS WHICH ARE OUTSIDE OF THE PROJECT'S AREAS OF DISTURBANCE AND WHICH ARE IMPACTED BY CONSTRUCTION OPERATIONS INCLUDING THOSE AREAS WHERE VEHICLES, EQUIPMENT AND MATERIALS ARE STORED WITH THE PERMISSION OF THE ENGINEER.
9. UNDER NO CIRCUMSTANCE WILL THE CONTRACTOR BE ALLOWED TO STOCKPILE REMOVED PAVEMENT MATERIALS WITHIN THE PROJECT LIMITS.
10. CLEANING AND SWEEPING OF PAVEMENT WILL INCLUDE REMOVAL OF ALL PAVEMENT DEBRIS PRIOR TO THE PLACEMENT OF EACH BITUMINOUS PAVEMENT LIFT. ALL CLEANING AND SWEEPING SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER.
11. PRIOR TO INSTALLATION, ALL SIGNS, MOUNTINGS AND LOCATIONS SHALL BE APPROVED OR MODIFIED BY THE ENGINEER.
12. THE COORDINATE SYSTEM, IF SHOWN, IS THE RHODE ISLAND STATE PLANE COORDINATE SYSTEM.
13. PAVEMENT OPERATIONS FOR CURBED SECTIONS: IN AREAS WHERE CURBING IS SET TO FINISH LINE AND GRADE, THE CONTRACTOR WILL NOT BE REQUIRED TO UTILIZE THE SENSOR AND SKY-TYPE DEVICE FOR AUTOMATIC GRADE CONTROL, BUT WILL BE ALLOWED TO MANUALLY ADJUST THE BITUMINOUS PAVER FOR CONTROLLING GRADE.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL ROADWAYS FREE OF DEBRIS RESULTING FROM THEIR CONSTRUCTION OPERATIONS. ALL DEBRIS SHALL BE REMOVED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE STATE.
15. NO FUEL STORAGE, VEHICLE REFUELING, OR EQUIPMENT STORAGE SHALL TAKE PLACE IN DESIGNATED WETLANDS, NOR WITHIN 100' OF ANY WATER BODY. THIS REQUIREMENT SHALL NOT SUPERSEDE ANY FEDERAL, STATE OR LOCAL LAW, ORDINANCE, RULE OR REGULATION THAT APPLIES TO THE SAME, UNLESS THIS REQUIREMENT IS MORE STRINGENT THAN SAID LAW, ORDINANCE, RULE OR REGULATION.
16. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT AT THE END OF FINAL PAVING OPERATIONS, FLOW TO EXISTING DRAINAGE STRUCTURES HAS BEEN REESTABLISHED AND THAT NO ISOLATED DEPRESSIONS REMAIN. THERE SHALL BE NO SEPARATE PAYMENT FOR THIS PROVISION; IT SHALL BE CONSIDERED INCIDENTAL TO PAVING AND COLD PLANING OPERATIONS.
17. ALL EMBANKMENTS SHALL BE PLACED IN HORIZONTAL LAYERS NOT EXCEEDING 12" (AFTER COMPACTION) AND SHALL BE COMPACTED AS SPECIFIED BEFORE THE NEXT LAYER IS PLACED. ALSO, EMBANKMENT CONSTRUCTION SHALL CONFORM TO SECTION 202.03.2 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
18. IF THIS PROJECT IS ON A HURRICANE EVACUATION AND DIVERSIONARY ROUTE, AS DESIGNATED ON THE COVERSHEET, THE CONTRACTOR IS ADVISED THAT UPON 12 (TWELVE) HOURS NOTICE THE ROADWAY SHALL BE OPEN TO EVACUEES AND EMERGENCY PERSONNEL. ANY EXTRA WORK NECESSARY TO COMPLY WITH THIS REQUIREMENT WILL BE REIMBURSED UNDER FORCE ACCOUNT PROCEDURES.
19. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS STATED IN THE ENVIRONMENTAL APPROVALS ISSUED FOR THE PROJECT FROM THE DEPARTMENT OF ENVIRONMENTAL MANAGEMENT (RIDEM). AND/OR THE ARMY CORPS OF ENGINEERS (ACOE). AND/OR THE COASTAL RESOURCES MANAGEMENT COUNCIL (CRMC). COPIES OF EACH OF THESE PERMITS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH THESE CONDITIONS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
20. FOR ALL PROJECTS INVOLVING KNOWN SITE REMEDIATION ISSUES, THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE CONSTRUCTION RELATED PROVISIONS, CONDITIONS, AND STIPULATIONS OF ANY REMEDIAL PLANS DEVELOPED FOR THE PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH COMPLIANCE WITH THESE DOCUMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
21. NO UNPROTECTED CONSTRUCTED FEATURE MAY PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. HEADWALL, DRAINAGE INLET, ETC.
22. THE REMAINING SECTION OR STUB OF A BREAKAWAY BASE MAY NOT PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. SIGN POSTS, LIGHT POLES, FIRE HYDRANTS, ETC.

DRAINAGE AND EROSION CONTROL NOTES:

1. FOR ALL PROJECTS WITH AT LEAST ONE(1) ACRE OF SOIL DISTURBANCE. R.I.D.O.T. IS REQUIRED TO DEVELOP AND ENFORCE A SITE SPECIFIC STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IN ORDER TO REMAIN IN COMPLIANCE WITH THE RIDDES GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS OF THE GENERAL PERMIT AND THE SITE SPECIFIC SWPPP FOR THIS PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH ADHERENCE TO THE SWPPP SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
2. NO UNDISTURBED AREAS SHALL BE CLEARED OF EXISTING VEGETATION AFTER OCTOBER 15 OF ANY CALENDAR YEAR OR DURING ANY PERIOD OF FULL OR LIMITED WINTER SHUTDOWN. ALL DISTURBED SOILS EXPOSED PRIOR TO OCTOBER 15 OF ANY CALENDAR YEAR SHALL BE SEEDED OR PROTECTED BY THAT DATE. ANY SUCH AREAS THAT DO NOT HAVE ADEQUATE VEGETATIVE STABILIZATION, AS DETERMINED BY THE RESIDENT ENGINEER OR ENVIRONMENTAL INSPECTOR, BY NOVEMBER 15 OF ANY CALENDAR YEAR, MUST BE STABILIZED THROUGH THE USE OF EROSION CONTROL MATTING OR HAY MULCH, IN ACCORDANCE WITH SPECIFICATIONS CONTAINED WITHIN THE R.I. SOIL EROSION AND SEDIMENT CONTROL HANDBOOK. IF WORK CONTINUES WITHIN ANY OF THESE AREAS DURING THE PERIOD FROM OCTOBER 15 THROUGH APRIL 15, CARE MUST BE TAKEN TO ENSURE THAT ONLY THE AREA REQUIRED FOR THAT DAY'S WORK IS EXPOSED, AND ALL ERODIBLE SOIL MUST BE REESTABLISHED WITHIN 5 WORKING DAYS. ANY WORK TO CORRECT PROBLEMS RESULTING FROM FAILURE TO COMPLY WITH THIS PROVISION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THERE WILL BE NO SEPARATE PAYMENT FOR THIS PROVISION, IT SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OPERATIONS. STABILIZATION OF ONE FORM OR ANOTHER AS DESCRIBED ABOVE SHALL BE ACHIEVED WITHIN 2 WEEKS OF FINAL GRADING.
3. STOCKPILES OF MATERIAL SHALL NOT BE LOCATED WITHIN REGULATED WETLANDS OR BUFFER ZONE AREAS. THEY SHALL HAVE SIDE SLOPES NO GREATER THAN 30% AND STOCKPILES OF ERODABLE MATERIAL SHALL ALSO BE SEEDED AND RINGED WITH R.I. STD. 9.1.0 TO STABILIZE.
4. IF THE PLANS INCLUDE SPECIFIC AREAS FOR PLACEMENT OF CONSTRUCTION DEWATERING BASINS AND/OR EQUIPMENT AND MATERIALS STORAGE AND STOCKPILING, AND IF THE CONTRACTOR ELECTS TO UTILIZE ANY OTHER AREAS FOR THESE PURPOSES, THIS SHALL BE APPROVED BY THE ENGINEER ONLY AFTER OBTAINING ANY NECESSARY PERMITS AND/OR PERMIT MODIFICATIONS FROM THE APPROPRIATE REGULATORY AUTHORITY(IES). ANY PERMITTING REQUIREMENTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE ACCOMPLISHED AT NO COST TO THE STATE. THE ENGINEER WILL COORDINATE SUBMISSION OF ANY REQUIRED PERMIT APPLICATION MATERIALS WITH THE R.I.D.O.T. OFFICE OF ENVIRONMENTAL PROGRAMS.
5. JUTE MESH SHALL BE USED TO STABILIZE PLANTABLE SOIL AND/OR LOAM IN ALL DITCHES, ON ALL SLOPES ADJACENT TO WETLANDS AND WETLAND PERIMETERS, AND ON ALL SLOPES WITHIN WATER QUALITY BASINS. JUTE MESH IN DITCHES SHALL EXTEND TO AN ELEVATION 2 FEET ABOVE THE BOTTOM OF THE DITCH.
6. SEEDING ON ALL SLOPES 3 TO 1 OR STEEPER SHALL CONSIST OF THE FOLLOWING APPLICATIONS UNLESS CHANGED IN THE CONTRACT.
 - a. SEEDING TYPE I.
 - b. ADHESIVE MULCH STABILIZER
7. UNVEGETATED SLOPES SHALL NOT BE UNATTENDED OR EXPOSED FOR PERIODS IN EXCESS OF 2 WEEKS OR THROUGH THE INACTIVE WINTER SEASON.
8. PRIOR TO DRAINAGE AND UTILITY CONSTRUCTION, THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE LOCATION (HORIZONTAL AND VERTICAL) OF ALL EXISTING PIPES AND/OR STRUCTURES WHICH ARE TO BE CONNECTED. ANY VARIATION FOUND FROM THE PLANS MUST BE BROUGHT TO THE ENGINEER'S ATTENTION PRIOR TO DRAINAGE AND UTILITY CONSTRUCTION. WORK CAN COMMENCE ONLY UPON THE ENGINEER'S AUTHORIZATION.
9. ALL DRAINAGE AND UTILITY STRUCTURES WITHIN THE PAVED ROADWAY SHALL BE ADJUSTED TO GRADE WITH THE SURROUNDING PAVEMENT PRIOR TO THE WINTER SHUTDOWN.
10. DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING DRAINAGE AND RUNOFF FLOW DURING STORMS AND PERIODS OF RAINFALL THROUGHOUT THE WORK AREA.
11. CATCH BASIN RIM GRADES NOTED ON PLANS ARE DEPRESSED 0.1' LOWER THAN THE GUTTER GRADE. RIM ELEVATIONS SHOWN ARE FINAL GRADES. THE CONTRACTOR SHALL PLACE FRAMES AND GRATES 0.1' BELOW THE GRADE CONSTRUCTED IN THIS CONTRACT OR AS DIRECTED BY THE ENGINEER.
12. PROVISIONS FOR CLEARING TO ACCESS OUTFALLS DURING THE CLEANING AND FLUSHING OF THE CLOSED DRAINAGE SYSTEM SHALL BE KEPT TO A MINIMUM.
 - a. ANY VEGETATIVE CLEARING SHALL BE LIMITED TO BRUSH AND TREES LESS THAN 3" DIAMETER.
 - b. NO HEAVY EQUIPMENT MAY ENCR OACH UPON VEGETATED PERIMETER OR RIVERBANK WETLANDS AS WELL AS BIOLOGICAL WETLANDS.
13. THE CONTRACTOR SHALL INSTALL ALL EROSION CONTROL DEVICES FOR OUTLET PROTECTION PRIOR TO CLEANING AND FLUSHING STORM WATER DRAINAGE. EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL ALL FLUSHED SEDIMENTS ARE REMOVED. AT ALL OUTFALL LOCATIONS WHERE PIPES ARE TO BE CLEANED AND FLUSHED, OUTLET PROTECTION (R.I. STD. 9.1.0 OR 9.3.0) SHALL BE INSTALLED TO TRAP SEDIMENTS. THESE SEDIMENTS SHALL THEN BE REMOVED AND DISPOSED OF LEGALLY BEFORE THE OUTLET PROTECTION DEVICES ARE REMOVED. IF OUTLET PROTECTION AT THE OUTFALL IS NOT FEASIBLE, THEN THE OUTLET PIPE OF THE LAST DRAINAGE STRUCTURE TO BE CLEANED SHALL BE PLUGGED TO CAPTURE ALL MATERIALS FLUSHED FROM PIPES. AFTER THE MATERIALS ARE REMOVED FROM THE DRAINAGE STRUCTURE, THE OUTLET SHALL BE UNPLUGGED TO RESUME NORMAL FUNCTIONING.
14. R.I. STD. 9.8.0 BALED HAY INLET PROTECTION SHALL BE INSTALLED AT ALL CATCH BASINS AND INLETS WHENEVER SUBBASE IS EXPOSED, AND SHALL REMAIN IN PLACE UNTIL THE ABUTTING GROUND SURFACES ARE STABILIZED.
15. WHERE BALED HAY INLET PROTECTION AND SILT FENCES ARE USED AT CATCH BASINS, THEY SHALL BE REMOVED AT THE END OF THE PROJECT OR AS DIRECTED BY THE ENGINEER IN ORDER TO PREVENT CLOGGING OF THE INLET.

DRAINAGE AND EROSION CONTROL NOTES (CONTINUED):

16. DETENTION AND RETENTION BASINS MAY BE ROUGH GRADED AND STABILIZED WITH VEGETATION AND/OR OTHER EROSION CONTROL MEASURES AS REQUIRED BY THE ENGINEER PRIOR TO USE AS TEMPORARY SEDIMENTATION BASINS DURING PROJECT CONSTRUCTION. FINAL BASIN CONSTRUCTION SHALL NOT COMMENCE UNTIL ALL SOURCES OF SEDIMENT HAVE BEEN ELIMINATED. FINAL ROADSIDE VEGETATION IS ESTABLISHED AND USE OF TEMPORARY BASINS IS NO LONGER REQUIRED AS DIRECTED BY THE ENGINEER. ANY ISSUES RELATING TO EROSION AND/OR SEDIMENT TRANSPORT INTO WETLAND AREAS RESULTING FROM SUCH USE OF SEDIMENTATION BASINS DURING CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ANY CORRECTIVE ACTION REQUIRED TO RESOLVE SUCH ISSUES SHALL BE COMPLETED BY THE CONTRACTOR.
17. THE TOE OF ANY FILL SLOPE IS TO REMAIN AT LEAST 1' INSIDE OF ALL EROSION CONTROLS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR COVER ANY PORTION OF THE EROSION CONTROL MEASURES WITH MATERIAL. ANY MATERIAL THAT IS PLACED ON ANY EROSION CONTROLS BY THE CONTRACTOR, OR ANY AGENT OF THE CONTRACTOR, SHALL BE IMMEDIATELY REMOVED BY THE CONTRACTOR, AND ANY NECESSARY REPAIRS TO THE EROSION CONTROLS ACCOMPLISHED.
18. PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES, EROSION AND SEDIMENTATION CONTROLS SHALL BE INSTALLED AT THOSE AREAS INDICATED ON THE PLANS. CLEARING MAY OCCUR PRIOR TO INSTALLATION OF SUCH CONTROLS, HOWEVER NO GRUBBING, GRADING, FILLING, OR OTHER SOIL DISTURBANCE SHALL OCCUR PRIOR TO INSTALLATION. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS.
19. ALL HAY BALES, SILT FENCE OR TEMPORARY PROTECTION SHALL REMAIN IN PLACE UNTIL AN ACCEPTABLE STAND OF GRASS IS ESTABLISHED. IF NEEDED, TEMPORARY SEEDING CAN HELP TO MINIMIZE EROSION. TEMPORARY SEED WILL CONFORM TO R.I.D.O.T. STANDARD TEMPORARY SEED MIX.
20. THE CONTRACTOR MUST REPAIR AND/OR RESEED ANY AREAS THAT DO NOT DEVELOP WITHIN THE PERIOD OF ONE YEAR AND HE SHALL DO SO AT NO ADDITIONAL EXPENSE TO THE STATE.
21. THE NORMAL ACCEPTABLE SEASONAL SEEDING DATES ARE SPECIFIED IN SUBSECTION L.02.03 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
22. ADDITIONAL EROSION CONTROLS, SHALL BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER. THESE ADDITIONAL ITEMS WILL BE PAID AT THE UNIT PRICE FOR THAT BID ITEM.

UTILITY NOTES:

1. EXISTING UTILITIES HAVE BEEN SHOWN ON THE PLANS USING THE BEST AVAILABLE INFORMATION AND ARE APPROXIMATE. BUILDING SERVICE CONNECTIONS (ELECTRIC, GAS, TELEPHONE, WATER AND SANITARY) ARE NOT SHOWN. CONTRACTOR IS TO ASSUME SERVICES ARE PRESENT TO ALL BUILDINGS.
2. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL EXISTING DRAINAGE AND UTILITIES BOTH UNDERGROUND AND OVERHEAD BEFORE EXCAVATION BEGINS IN ACCORDANCE WITH CHAPTER 39-1.2 OF THE R.I. GENERAL LAWS ENTITLED "EXCAVATION NEAR UNDERGROUND UTILITY FACILITIES", WITH AMENDMENTS EFFECTIVE AS OF NOVEMBER 1, 2009 AND, WHEN NECESSARY, BY CONTACTING THE INDIVIDUAL UTILITY COMPANIES. EXCAVATION SHALL BE IN ACCORDANCE WITH ALL STATUTES, ORDINANCES, RULES AND REGULATIONS OF ANY APPLICABLE CITY, TOWN, STATE OR FEDERAL AGENCY. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE DIG SAFE PROGRAM. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES AND ENSURE THAT ALL UTILITIES HAVE BEEN MARKED PRIOR TO COMMENCING THEIR WORK. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANY, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.
3. ALL EXISTING UTILITIES TO BE ABANDONED SHALL BE CAPPED.
4. EXISTING WATER SERVICES SHALL BE RECONNECTED TO THE NEW WATER MAINS.
5. UTILITY SERVICE CONNECTIONS SHALL BE MAINTAINED TO ALL EXISTING FACILITIES TO REMAIN.
6. FIRE HYDRANTS SHALL NOT BE REMOVED FROM SERVICE WITHOUT WRITTEN AUTHORIZATION FROM THE FIRE DEPARTMENT OR THE WATER AUTHORITY.
7. ALL NEW WATER LINES SHALL BE DISINFECTED TO THE SATISFACTION OF THE WATER AUTHORITY IN ACCORDANCE WITH THE SPECIFICATIONS.
8. ALL UTILITY POLE RELATED WORK SHALL BE BY OTHERS.

**Proposed
Airport Rd/
Commerce Drive
Intersection
Improvements**

Warwick, Rhode Island

| No. | Revision | Date | Appr. |
|-----|---------------------|-----------|-------|
| 1 | Final Plan Approval | 2/22/2022 | |

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|---------------------|-------------------------|
| Designed by | Checked by |
| Issued for | Date |
| Construction | January 28, 2022 |

Standard Notes - 1

Drawing Number

C-4

Sheet 4 of 14

Project Number
73180.00

LANDSCAPE NOTES:

1. ALL PLANT MATERIAL MUST BE TAGGED AT THE NURSERY (A RECOGNIZED GROWER OF PLANT MATERIAL) IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION. ALL PLANT MATERIAL MUST BE NURSERY GROWN; NO PLANTATION GROWN PLANT MATERIAL WILL BE ACCEPTED.
2. ALL PLANT SUBSTITUTIONS AND/OR CHANGES IN PLANT LOCATION MUST BE APPROVED IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
3. ALL PLANT MATERIAL IS TO BE FIELD LOCATED BY A REPRESENTATIVE FROM THE R.I.D.O.T. LANDSCAPE ARCHITECTURE UNIT.
4. A R.I.D.O.T. LANDSCAPE REPRESENTATIVE MUST BE ON SITE TO APPROVE ALL TRIMMING AND CLEARING NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS.
5. ANY TOPSOIL USED AS PLANTABLE SOIL SHALL HAVE A SANDY LOAM TEXTURE RELATIVELY FREE OF SUBSOIL MATERIAL, STONES, ROOTS, LUMPS OF SOIL, TREE LIMBS, TRASH OR CONSTRUCTION DEBRIS, AND SHALL CONFORM TO SECTION M.18 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
6. ALL TREES AND SHRUBS SHALL BE MULCHED WITH PINE BARK MULCH IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
7. ALL TREES AND/OR SHRUBS THAT ARE PLANTED AS A BED SHALL BE MULCHED AS A BED.
8. PROVIDE A MINIMUM 6"-8" BRANCHING STANDARD ON ALL TREES INSTALLED ADJACENT TO SIDEWALKS AND/OR PEDESTRIAN ACCESS AREAS.

STRUCTURAL NOTES FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS:

GENERAL

1. ALL SUPPORT DESIGNS AND ASSOCIATED SHOP DRAWING REVIEWS SHALL BE IN CONFORMANCE WITH THE LATEST EDITION, OF THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS (THE "SPECIFICATIONS"), INCLUDING THE LATEST INTERIM SPECIFICATIONS, EXCEPT AS MODIFIED HEREIN.

CONSTRUCTION DRAWINGS AND DETAILS

1. THE FOLLOWING NOTES SHALL BE INCLUDED ON ALL PLANS AND/OR SHOP DRAWINGS IN REFERENCE TO ANCHOR BOLTS:
 - "PRETENSIONING OF ALL ANCHOR NUTS IS REQUIRED, AND SHALL BE ACCOMPLISHED BY TIGHTENING TO 1/6TH TURN BEYOND THE SNUG-TIGHT POSITION."
 - "THE MAXIMUM CLEARANCE BETWEEN THE BOTTOM OF THE LEVELING NUTS AND THE TOP OF THE CONCRETE IS CRITICAL AND SHALL NOT EXCEED THE AMOUNT SPECIFIED ON THIS DRAWING."
2. THE USE OF GROUT UNDER BASE PLATES SHALL GENERALLY NOT BE PERMITTED. IF SPECIFIC CONDITIONS WARRANT ITS USE, THE GROUT SHALL NOT BE CONSIDERED LOAD CARRYING; LOADS SHALL BE DIRECTLY SUPPORTED BY THE ANCHOR BOLTS. ADEQUATE DRAINAGE SHALL BE PROVIDED.
3. THE DAMPENING EFFECTS OF VIBRATION MITIGATION DEVICES SHALL NOT BE CONSIDERED IN THE DESIGN OF STRUCTURAL SUPPORTS FOR SIGNS AND TRAFFIC SIGNALS. IF THE CONTRACTOR CHOOSES TO USE THESE DEVICES FOR WARRANTY PURPOSES, THE TYPE OF DEVICES PROPOSED SHALL BE APPROVED BY THE DEPARTMENT PRIOR TO FABRICATION OF SUPPORTS.

TRAFFIC SIGNAL NOTES:

1. ALL SALVAGED TRAFFIC SIGNAL EQUIPMENT SHALL BE DELIVERED TO THE R.I.D.O.T. MAINTENANCE HEADQUARTERS, 360 LINCOLN AVENUE, WARWICK, RHODE ISLAND, 02888.
2. BACK PLATES SHALL BE INSTALLED ON ALL TRAFFIC SIGNAL HEADS.
3. THE CONTRACTOR SHALL SUPPLY AND INSTALL ON THE UPPER LEFT HAND CORNER OF THE BACK OF THE CONTROLLER CABINET DOOR A LAMINATED INTERSECTION GRAPHIC AND TABLE DEPICTING THE TRAFFIC DETECTOR RELAY CHANNEL ASSIGNMENTS. THE DIAGRAM SHALL BE A GRAPHIC OF THE INDIVIDUAL INTERSECTION ORIENTED SIMILAR TO THE PLANS SHOWING THE LOCATIONS OF EACH OF THE LOOP DETECTORS. THE DIAGRAM SHALL, AT A MINIMUM, INCLUDE DETECTOR NUMBERS, STREET NAME LABELS, NORTH ARROW, AND CONTROLLER CABINET LOCATION. THE ASSIGNMENT INFORMATION SHALL BE INCLUDED IN A TABLE WHICH SHALL INCLUDE, AT A MINIMUM, THE APPROACH NAME, DETECTOR NUMBER, TERMINAL NUMBER, DETECTOR RACK SLOT NUMBER, RELAY NUMBER, RELAY CHANNEL NUMBER, AND PHASE ASSOCIATED WITH EACH DETECTOR.
4. TRAFFIC CONTROLLER CABINETS, UNLESS OTHERWISE NOTED, SHALL BE NEMA TS2 TYPE 1 CABINET SIZE 6 ("P" TYPE) WITH NOMINAL DIMENSIONS OF 52"Hx44"Wx24"D.
5. ALL DELAY AND EXTENSION TIMES, AS CALLED FOR ON THE PLANS, FOR PROPOSED LOOP DETECTORS SHALL BE PROGRAMMED IN THE TRAFFIC SIGNAL CONTROLLER AND NOT THE DETECTOR RELAY.
6. A BARE GROUND WIRE SHALL BE PLACED IN ALL PVC CONDUITS AND SHALL BE BONDED TO GROUND RODS IN ACCORDANCE WITH SECTION T.03 OF THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
7. THE FINAL POSITION OF SIGNAL HEADS, PEDESTRIAN PUSHBUTTONS, DETECTORS, AND STOP LINE AND CROSSWALK PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER IN THE FIELD ACCORDING TO ACTUAL INTERSECTION CHARACTERISTICS.
8. A 2' MINIMUM BUFFER SHALL BE PROVIDED BETWEEN THE CURB AND ALL LATERAL OBSTRUCTIONS (INCLUDING ALL SIGNAL POLES AND TRAFFIC/PEDESTRIAN SIGNAL HEADS) TO PROVIDE ADEQUATE CLEARANCE FOR TURNING VEHICLES.
9. ALL FOUNDATIONS MUST HAVE CONES OR BARRELS BOLTED TO FOUNDATION BASES UNTIL ACTUAL POLE IS INSTALLED.
10. WHEN PLACING TRAFFIC SIGNAL HANDHOLES OR CONDUIT IN EXISTING PORTLAND CEMENT CONCRETE SIDEWALKS, THE ENTIRE SIDEWALK SQUARE OF CONCRETE SHALL BE REPLACED IN ACCORDANCE WITH R.I. STD. 43.1.0. NO PATCHES WILL BE ALLOWED.
11. ALL PEDESTRIAN PUSHBUTTONS SHALL BE COMPLIANT WITH "THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES" (ADAAG) AND SHALL INCLUDE A PRESSURE-ACTIVATED (NON-MOVING) BUTTON. SIGNS APPLICABLE TO PUSHBUTTON ACTUATION SHALL BE INSTALLED SUCH THAT THE CROSSING ASSIGNED TO EACH BUTTON IS CLEARLY INDICATED. IF SITE CONDITIONS DO NOT ALLOW PEDESTRIAN PUSHBUTTONS TO BE INSTALLED WHERE CALLED FOR ON THE PLANS, THE R.I.D.O.T. TRAFFIC ENGINEERING UNIT SHALL BE CONSULTED WITH THROUGH AN R.F.I. PRIOR TO INSTALLING THE PUSHBUTTONS. THE FINAL PLACEMENT OF ALL PEDESTRIAN PUSHBUTTONS SHALL BE IN ACCORDANCE WITH ADAAG AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
12. ALL LOOP DETECTORS SHALL BE CENTERED WITHIN EACH LANE AS DELINEATED, UNLESS OTHERWISE DIMENSIONED ON PLANS.
13. ALL LOOP DETECTORS SHALL BE CUT INTO THE FINAL PAVEMENT SURFACE COURSE.
14. TRAFFIC SIGNAL CONTROLLERS SHALL BE WIRED SO THAT ANY FIRE PRE-EMPTION SHALL OVERRIDE MANUAL (PUSH BUTTON) OPERATION.
15. THE CONTRACTOR SHALL WORK CONTINUOUSLY TO RESTORE TRAFFIC SIGNAL OPERATION TO ITS INTENDED PURPOSE WHEN REPLACING THE TRAFFIC SIGNAL EQUIPMENT. A POLICE DETAIL IS REQUIRED TO DIRECT TRAFFIC AT THE INTERSECTION AT ALL TIMES WHEN THE TRAFFIC SIGNAL IS INOPERATIVE. AT NO TIME SHALL THE CONTRACTOR LEAVE THE SITE BEFORE RESTORING FULL TRAFFIC OPERATIONS.

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

1. ALL MAINTENANCE AND PROTECTION OF TRAFFIC CONTROL SETUPS, SIGNS, CHANNELIZING DEVICES, ETC., SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
2. ALL SIGN MOUNTINGS FOR TEMPORARY AND CONSTRUCTION SIGNS SHALL BE IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
3. THE CONTRACTOR SHALL COVER ALL EXISTING AND/OR TEMPORARY SIGNS THAT ARE NOT RELEVANT TO THE TRAFFIC CONTROL REQUIRED DURING ANY PARTICULAR STAGE OF THE CONTRACT.
4. ADVANCE FLAGPERSON SIGNS (W20-7A) SHALL BE USED IN ADVANCE OF ANY POINT AT WHICH A FLAGPERSON OR A POLICE OFFICER HAS BEEN STATIONED TO CONTROL TRAFFIC. WHEN NEEDED, AN APPROPRIATE DISTANCE MESSAGE MAY BE DISPLAYED ON A SUPPLEMENTAL PLATE (24"x18") BELOW THE FLAGPERSON SYMBOL SIGN. THE SIGN SHALL BE PROMPTLY REMOVED OR COVERED WHENEVER THE FLAGPERSON IS NOT AT THE STATION.
5. POLICE OFFICERS (AND NOT FLAGPERSONS) SHALL BE UTILIZED WHEN WORK WILL IMPACT SIGNALIZED INTERSECTIONS AND LIMITED ACCESS HIGHWAYS.
6. POLYETHYLENE DRUMS SHALL BE UTILIZED AS A CHANNELIZING DEVICE WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN BEYOND WORKING HOURS WHEN NO WORKERS ARE PRESENT. CONES SHALL BE UTILIZED WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN ONLY DURING WORKING HOURS AND IS SUBSEQUENTLY BROKEN DOWN AT THE END OF THE WORKDAY.
7. ARROW PANELS SHALL BE SET IN THE FLASHING FOUR CORNERS CAUTION MODE UNLESS UTILIZED FOR A MERGING TAPER. ARROW PANELS SET IN THE FLASHING ARROW MODE SHALL NOT BE UTILIZED FOR LANE SHIFTS.
8. TEMPORARY CONSTRUCTION SIGNS AND OTHER WORKZONE TRAFFIC CONTROL DEVICES THAT ARE DAMAGED OR REQUIRE RELOCATION SHALL BE REPLACED AND / OR RELOCATED UNDER THE PAY ITEM FOR "MAINTENANCE AND MOVEMENT TRAFFIC PROTECTION."
9. THE PRIVATE VEHICLES OF CONSTRUCTION WORKERS SHALL NOT BE PARKED ON THE TRAVEL LANES OR SHOULDERS. THEY MAY BE PARKED WITHIN THE STATE RIGHT-OF-WAY ONLY IN AREAS 30' BEYOND THE OUTSIDE EDGE OF THE TRAVEL LANES AND/OR IN AREAS APPROVED BY THE ENGINEER.
10. TEMPORARY CONSTRUCTION SIGNS AND OTHER TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE START OF WORK IN ANY AREA OPEN TO TRAFFIC, AND SHALL BE REMOVED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER APPROPRIATE.
11. THE INTENDED VEHICLE PATHS THROUGH EACH WORK ZONE SHALL BE CLEARLY MARKED AT ALL TIMES. WATERBORNE PAVEMENT MARKINGS SHALL BE INSTALLED BEFORE THE END OF THE WORK SHIFT ON ALL COLD-PLANE AND NEW ROADWAY SURFACES THAT WILL BE OPENED TO TRAFFIC AT THE END OF THE SHIFT.



1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100

**Proposed
Airport Rd/
Commerce Drive
Intersection
Improvements**

Warwick, Rhode Island

| No. | Revision | Date | Apprv. |
|-----|---------------------|-----------|--------|
| 1 | Final Plan Approval | 2/22/2022 | |

| Designed by | Checked by |
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| | |
| Issued for | Date |
| Construction | January 28, 2022 |

Standard Notes - 2

Drawing Number

C-5

Sheet 5 of 14

Project Number
73180.00

JOB SPECIFIC LEGEND:

- 20.1.0 PAVEMENT MARKINGS ARROWS AND ONLY
- 20.3.0 PAVEMENT MARKINGS - CROSSWALKS AND STOP LINES (10' WIDTH)
- CFS COMPOST FILTER SOCK
- DSB REMOVE AND DISPOSE SHRUBS
- FDP FULL DEPTH PAVEMENT
2" CLASS 12.5 HMA MODIFIED
6" CLASS 19.0 HMA
12" GRAVEL BORROW SUBBASE COURSE
- IP INLET PROTECTION
- MM^(X*) MICROMILL (DEPTH)
- OV OVERLAY
2" CLASS 12.5 HMA MODIFIED
VARIABLE DEPTH CLASS 12.5 HMA MODIFIED FOR LEVELING
- RPS REMOVE AND RESET PRIVATE SIGN
- 6BW 6" BROKEN WHITE LANE LINE MARKING - SEE DETAIL
- 6DSW 6" DASHED WHITE LANE LINE MARKING - SEE DETAIL
- 8Y 8" EPOXY RESIN PAVEMENT MARKINGS - YELLOW

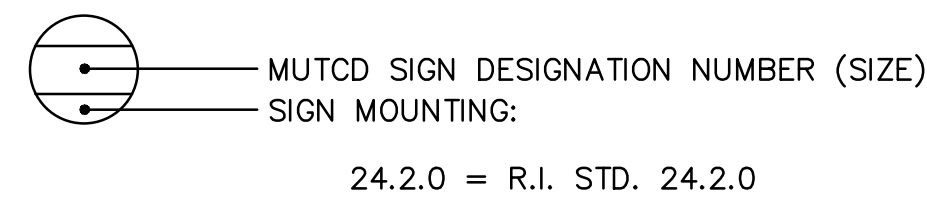
GENERAL NOTES - SIGNS:

- ALL NEW DIRECTIONAL, REGULATORY, WARNING, GUIDE SIGNS AND PARKING SIGNS SHALL HAVE SIGN SUPPORTS, UNLESS OTHERWISE INDICATED, SIGN MOUNTINGS SHALL BE R.I. STD. 24.2.0 OR 24.6.0 AS APPROPRIATE.
- PRIOR TO INSTALLATION, ALL SIGNS, MOUNTINGS AND LOCATIONS SHALL BE APPROVED OR MODIFIED BY THE RIDOT.
- ALL PROPOSED AND RELOCATED SIGNS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 7' OVER THE SIDEWALK. FOR SIGNS TO BE RELOCATED, IF 7' OF CLEARANCE CANNOT BE ACHIEVED USING THE EXISTING POST, A NEW POST SHALL BE USED. THE COST OF THE NEW POST SHALL BE CONSIDERED INCIDENTAL TO ITEM T15.0200 - REMOVE AND RELOCATE DIRECTIONAL REGULATORY OR WARNING SIGN.
- ALL SIGN RADII AND BORDERS SHALL BE AS SPECIFIED IN THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AS AMENDED.

GENERAL NOTES - PAVEMENT MARKINGS:

- ALL PERMANENT PAVEMENT MARKINGS FOR THIS PROJECT SHALL BE EPOXY RESIN.
- THE LOCATION OF PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) 2009 EDITION, AS AMENDED.
- WHERE EXISTING PAVEMENT MARKINGS CONFLICT WITH PROPOSED PAVEMENT MARKINGS, EXISTING MARKINGS SHALL BE REMOVED BY METHOD APPROVED BY THE RIDOT.
- LIMITS OF PROPOSED PAVEMENT MARKINGS SHALL MEET EXISTING STRIPING, UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL VERIFY AND RECORD PAVEMENT MARKING LOCATIONS PRIOR TO ANY PAVEMENT REMOVAL. MARKINGS SHALL BE REPLACED IN ORIGINAL LOCATIONS UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

TYPICAL SIGN DESIGNATION SYMBOL



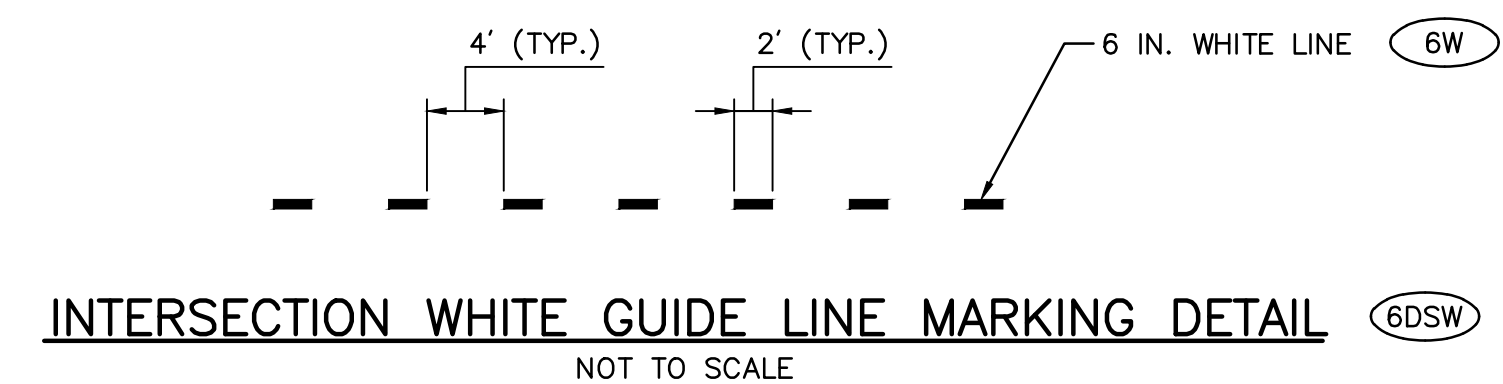
TRAFFIC SIGNAL PLAN SYMBOL LEGEND

- | | | | |
|--|---------------------------------|--|------------------------------------|
| | EXISTING CONTROLLER CABINET | | EXISTING MAST ARM POLE |
| | PROPOSED CONTROLLER CABINET | | PROPOSED MAST ARM POLE |
| | EXISTING HANDHOLE | | EXISTING LOOP DETECTOR |
| | PROPOSED HANDHOLE | | PROPOSED LOOP DETECTOR |
| | EXISTING TRAFFIC SIGNAL HEAD | | EXISTING OVERHEAD SIGN |
| | PROPOSED TRAFFIC SIGNAL HEAD | | PROPOSED OVERHEAD SIGN |
| | EXISTING PEDESTRIAN SIGNAL HEAD | | PROPOSED THERMAL IP VIDEO DETECTOR |
| | PROPOSED PEDESTRIAN SIGNAL HEAD | | EXISTING VIDEO DETECTION ZONE |
| | EXISTING PEDESTRIAN PUSH BUTTON | | PROPOSED VIDEO DETECTION ZONE |
| | PROPOSED PEDESTRIAN PUSH BUTTON | | PROPOSED SPAN OR PEDESTAL POLE |
| | EXISTING CONDUIT | | |
| | PROPOSED CONDUIT | | |
| | EXISTING SPAN OR PEDESTAL POLE | | |

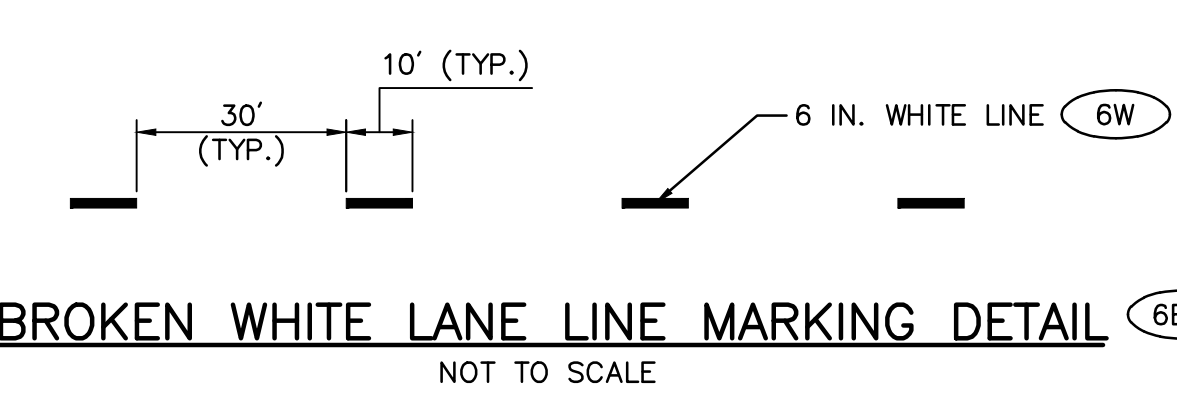
GENERAL NOTES - TRAFFIC SIGNALS:

- IF THE ASSIGNED RIGHT OF WAY FOR ANY TRAFFIC MOVEMENT IS TO REMAIN IN EFFECT DURING THE NEXT CALLED PHASE, THE SIGNAL INDICATIONS FOR THAT TRAFFIC MOVEMENT WILL NOT CHANGE DURING THE CLEARANCE INTERVAL.
- THE RIGHT OF WAY MAY BE ASSIGNED TO ANY PHASE, OR COMBINATION OF NON-CONFLICTING PHASES.
- ACCESS TO PEDESTRIAN PUSHBUTTONS SHALL MEET PROWAG & 2009 MUTCD REQUIREMENTS. ALL PEDESTRIAN PUSHBUTTONS SHALL BE ADA COMPLIANT WITH A MINIMUM 2" DIAMETER. SIGNS INSTALLED AT PROPOSED PEDESTRIAN PUSHBUTTONS SHALL BE MUTCD 2009 CODE R10-3E (LEFT OR RIGHT) AND SHALL BE INSTALLED SO THAT IT IS CLEARLY INDICATED WHICH CROSSING IS ASSIGNED TO EACH BUTTON. (THIS NOTE SHALL SUPPLEMENT TRAFFIC SIGNAL NOTE 11 ON STANDARD NOTES--2)
- CONTRACTOR SHALL HAND DIG AT ALL GAS PIPELINE CROSSINGS.
- A ROTARY WHEEL OR OTHER MECHANICAL TRENCHING METHOD SHALL NOT BE ALLOWED.
- A 1 FOOT VERTICAL CLEARANCE IS REQUIRED BETWEEN NEW CONDUIT AND NATIONAL GRID GAS FACILITIES.
- ALL NEW MANHOLES, HANDHOLES, PULL BOXES, AND FOUNDATIONS SHALL MEET A MINIMUM 1 FOOT LATERAL CLEARANCE AND NOT BE PLACED OVER NATIONAL GRID GAS AND WATER DEPARTMENT FACILITIES.
- PRIOR TO ORDERING AND/OR FABRICATION OF THE MAST ARMS AND PEDESTAL POLES, THE CONTRACTOR SHALL PERFORM TEST PITS TO CONFIRM THAT THERE ARE NO OBSTRUCTIONS WITH THE PROPOSED FOUNDATIONS. IF OBSTRUCTIONS ARE ENCOUNTERED, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER. THE ENGINEER, THE CONTRACTOR, AND THE ENGINEER OF RECORD WILL WORK TOGETHER ON IDENTIFYING AN ALTERNATE LOCATION FOR THE NEW MAST ARM OR PEDESTAL POLE. PLEASE NOTE THAT THESE ALTERNATE LOCATIONS MAY RESULT IN LONGER MAST ARMS OR FEWER PEDESTAL POLES THAN WHAT IS SHOWN ON THE PLANS. THE ENGINEER WILL DIRECT THE CONTRACTOR TO INCORPORATE THE APPROPRIATE REVISIONS IN THE CONTRACT.
- ALL APS PUSHBUTTONS SHALL BE DRY FIT IN THE PRESENCE OF THE ENGINEER OR HIS REPRESENTATIVE TO DEMONSTRATE PROPER ORIENTATION OF THE PUSHBUTTON'S TACTILE ARROW/R10-3e SIGN ARROW WHICH IS TO BE ALIGNED PARALLEL TO THE CROSSWALK DIRECTION.
- ALL CABLE DRIP LOOPS SHALL UTILIZE UV RATED CABLE TIES. THE USE OF ELECTRICAL TAPE WILL NOT BE ALLOWED.
- AN INDIVIDUAL CONTROL UNIT FOR EACH ACCESSIBLE PEDESTRIAN DETECTOR SHALL BE INSTALLED IN THE ASSOCIATED PEDESTRIAN SIGNAL HEAD OR IN THE DETECTOR HOUSING ITSELF. A CENTRAL CONTROL UNIT IN THE TRAFFIC SIGNAL CONTROLLER CABINET FOR THE ACCESSIBLE PEDESTRIAN DETECTORS SHALL NOT BE FURNISHED OR INSTALLED.
- THE ASSUMED LOCATIONS OF EXISTING CONDUIT NETWORKS SHOWN ON THE PLANS ARE BASED ON HISTORICAL TRAFFIC SIGNAL PLANS.
- THE CONTRACTOR SHALL WORK CONTINUOUSLY TO RESTORE TRAFFIC SIGNAL OPERATION TO ITS INTENDED PURPOSE WHEN REPLACING THE TRAFFIC SIGNAL EQUIPMENT. A POLICE DETAIL IS REQUIRED TO DIRECT TRAFFIC AT THE INTERSECTION AT ALL TIMES WHEN THE TRAFFIC SIGNAL IS INOPERATIVE. AT NO TIME SHALL THE CONTRACTOR LEAVE THE SITE BEFORE RESTORING FULL TRAFFIC OPERATIONS.

NOTES:
1. CONTRACTOR TO COORDINATE WORK WITH COMMERCE DRIVE UTILITY IMPROVEMENTS AND PAVEMENT MILL AND OVERLAY PLANS.



INTERSECTION WHITE GUIDE LINE MARKING DETAIL (6W)



BROKEN WHITE LANE LINE MARKING DETAIL (6BW)

Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island

| No. | Revision | Date | App'd. |
|-----|---------------------|-----------|--------|
| 1 | Final Plan Approval | 2/22/2022 | |

| | |
|--------------|------------------|
| Designed by | Checked by |
| Issued for | Date |
| Construction | January 28, 2022 |

Job Specific Plan Symbols, Legend & Notes

Drawing Number

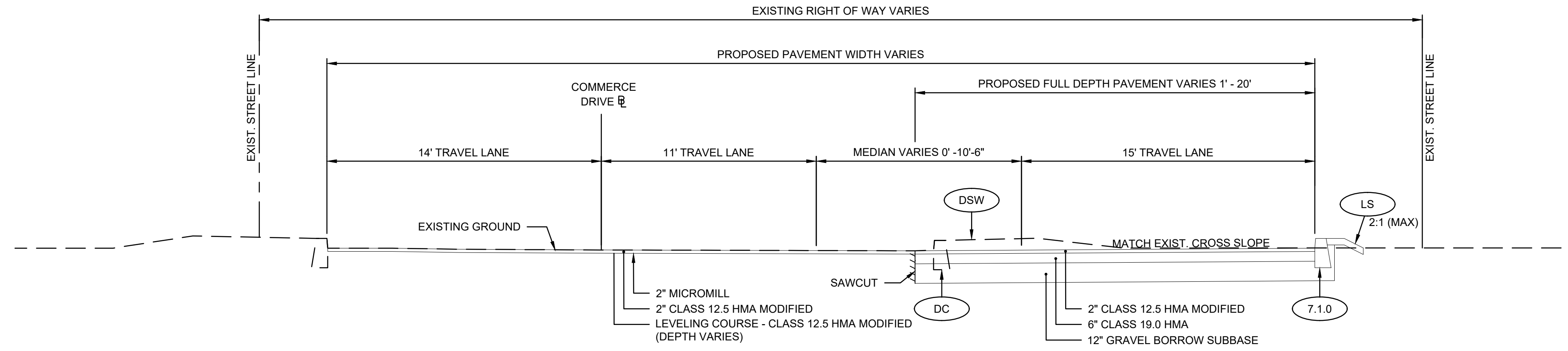
C-6

Sheet 6 of 14

Project Number
73180.00

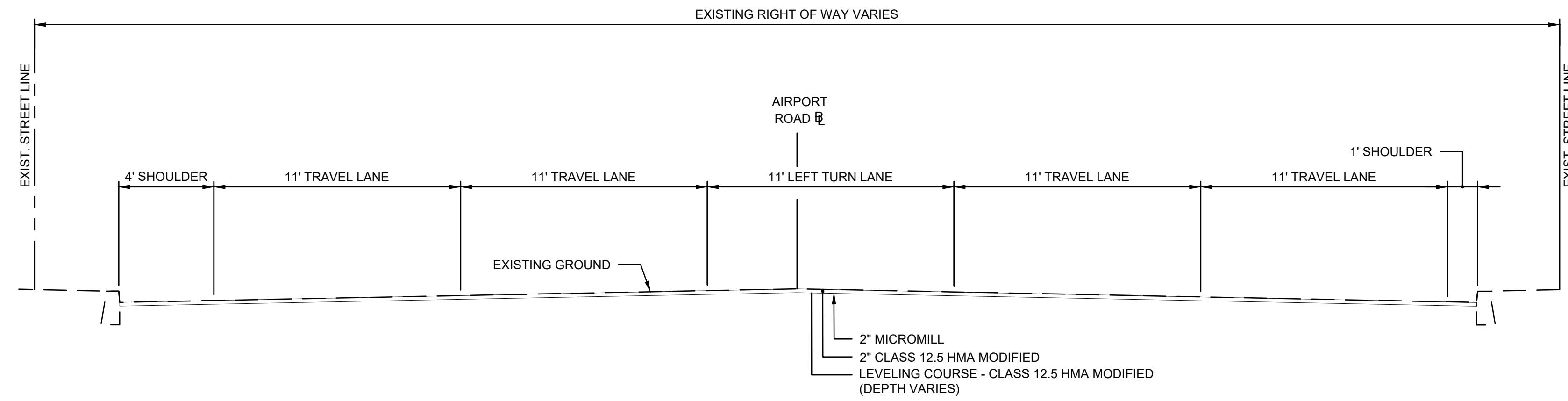


1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100



NOTE:
1. CONCRETE CURB LOCK SHALL BE USED WHEN FULL DEPTH PAVEMENT WIDENING WIDTH IS LESS THAN 3'.

STATION 100+50 TO STATION 102+55
COMMERCE DRIVE
SCALE: 1"=4'



STATION 23+25 TO STATION 25+25
AIRPORT ROAD
SCALE: 1"=4'

Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island

| No. | Revision | Date | Appvd. |
|-----|---------------------|-----------|--------|
| 1 | Final Plan Approval | 2/22/2022 | |

| Designed by | Checked by |
|-------------|------------|
| | |

Issued for: Construction Date: January 28, 2022

Typical Sections

Drawing Number

C-7

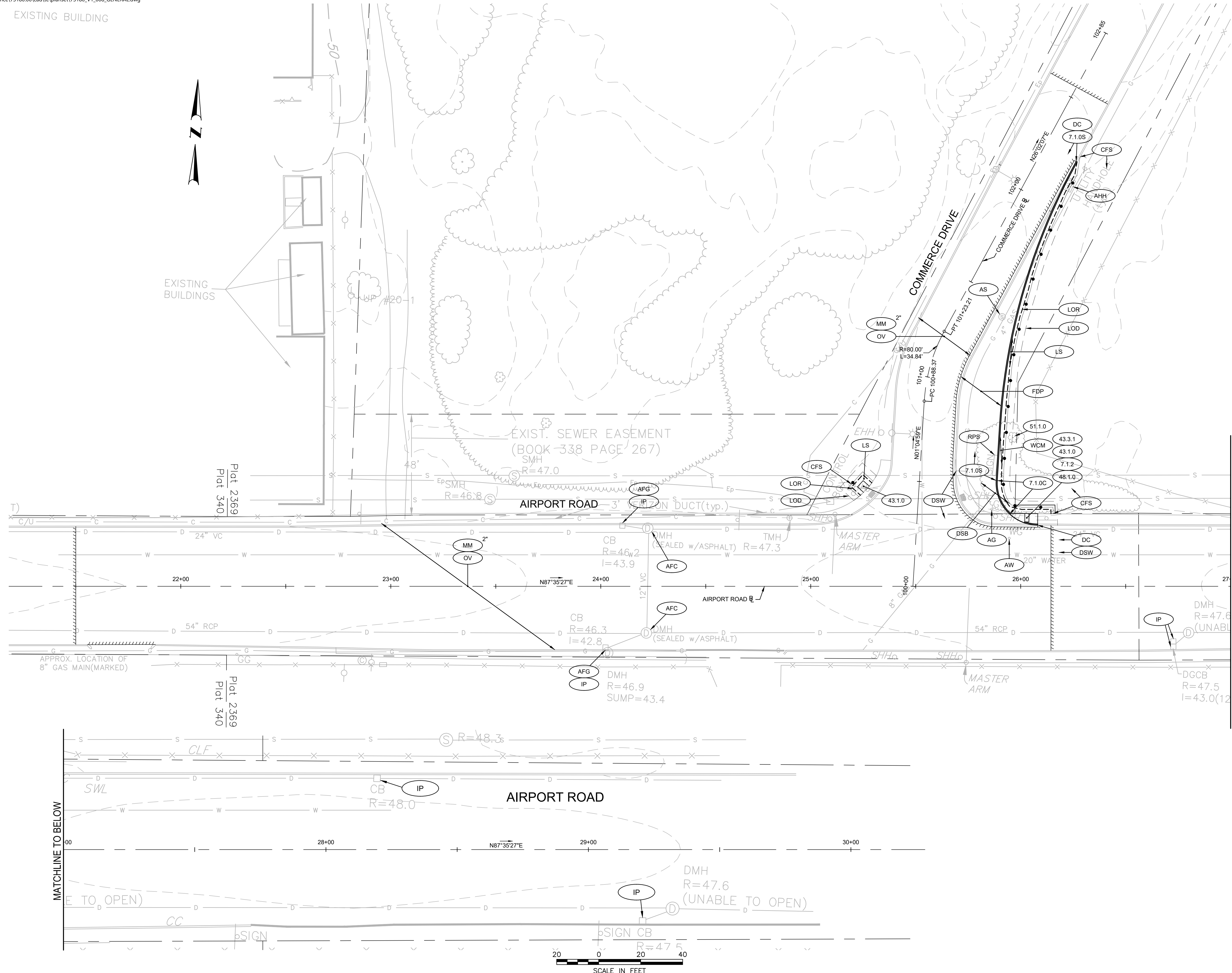
Sheet 7 of 14

Project Number
73180.00

EXISTING BUILDING



1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100



NOTES:
1. CONTRACTOR TO
COORDINATE WORK WITH
COMMERCE DRIVE UTILITY
IMPROVEMENTS AND
PAVEMENT MILL AND OVERLAY
PLANS.

Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island

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|-----|---------------------|-----------|--------|
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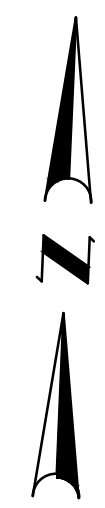
Construction January 28, 2022

General Plan

Drawing Number
C-8
 Sheet 8 of 14
 Project Number
73180.00

Saved Wednesday, January 26, 2022 2:44:23 PM TMELLO Plotted Wednesday, January 26, 2022 4:12:04 PM Tanya Nello

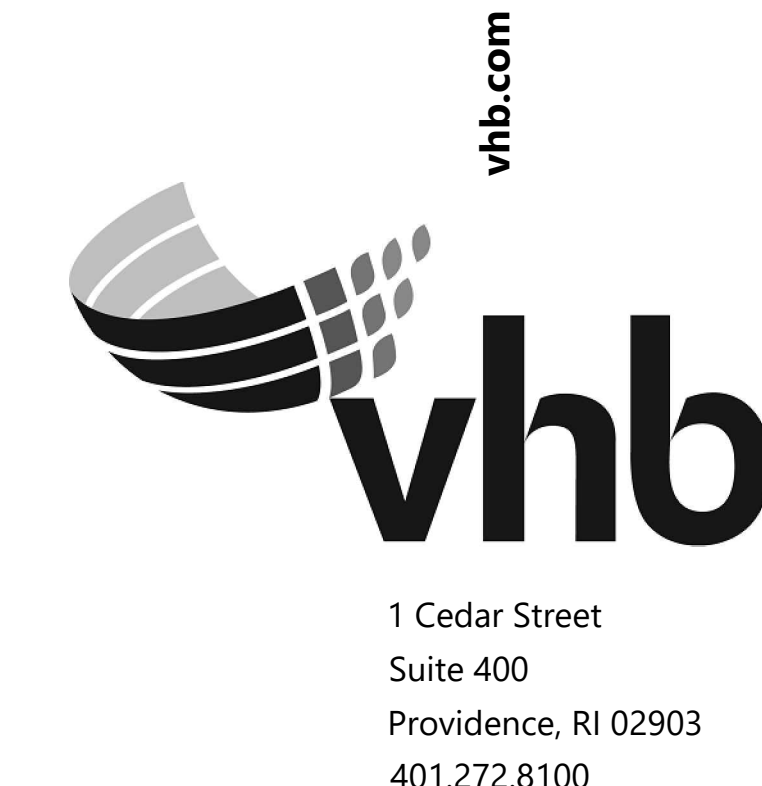
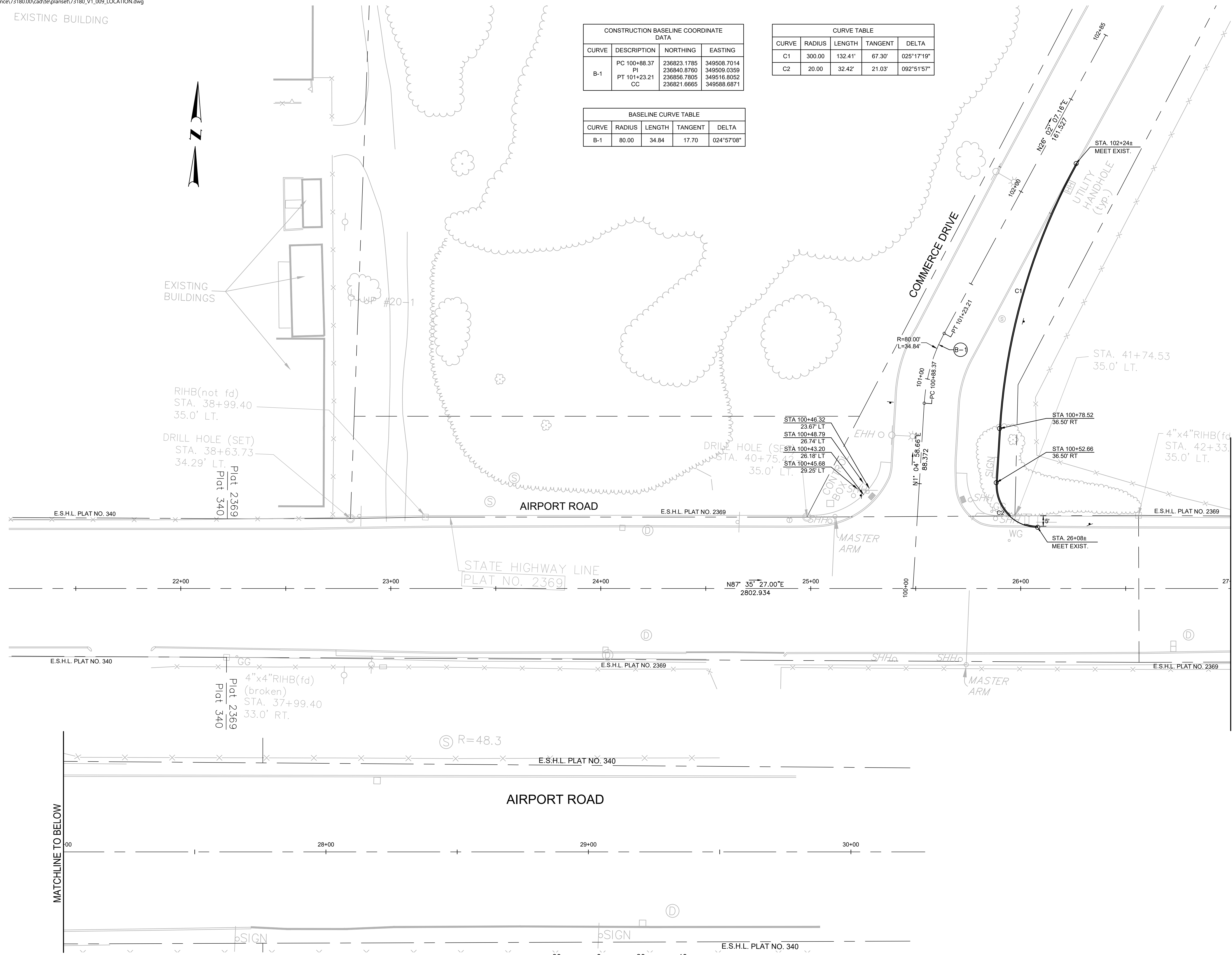
EXISTING BUILDING



| CONSTRUCTION BASELINE COORDINATE DATA | | | |
|---------------------------------------|--------------|-------------|-------------|
| CURVE | DESCRIPTION | NORTHING | EASTING |
| B-1 | PC 100+88.37 | 236823.1785 | 349508.7014 |
| | PI | 236840.8760 | 349509.0359 |
| | PT 101+23.21 | 236855.7805 | 349516.8052 |
| | CC | 236821.6665 | 349588.6871 |

| CURVE TABLE | | | | |
|-------------|--------|---------|---------|------------|
| CURVE | RADIUS | LENGTH | TANGENT | DELTA |
| C1 | 300.00 | 132.41' | 67.30' | 025°17'19" |
| C2 | 20.00 | 32.42' | 21.03' | 092°51'57" |

| BASELINE CURVE TABLE | | | | |
|----------------------|--------|--------|---------|------------|
| CURVE | RADIUS | LENGTH | TANGENT | DELTA |
| B-1 | 80.00 | 34.84 | 17.70 | 024°57'08" |



Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island

| No. | Revision | Date | Appr. |
|-----|---------------------|-----------|-------|
| 1 | Final Plan Approval | 2/22/2022 | |

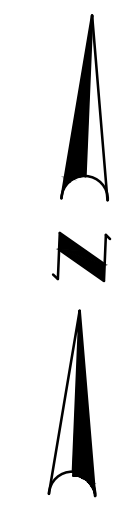
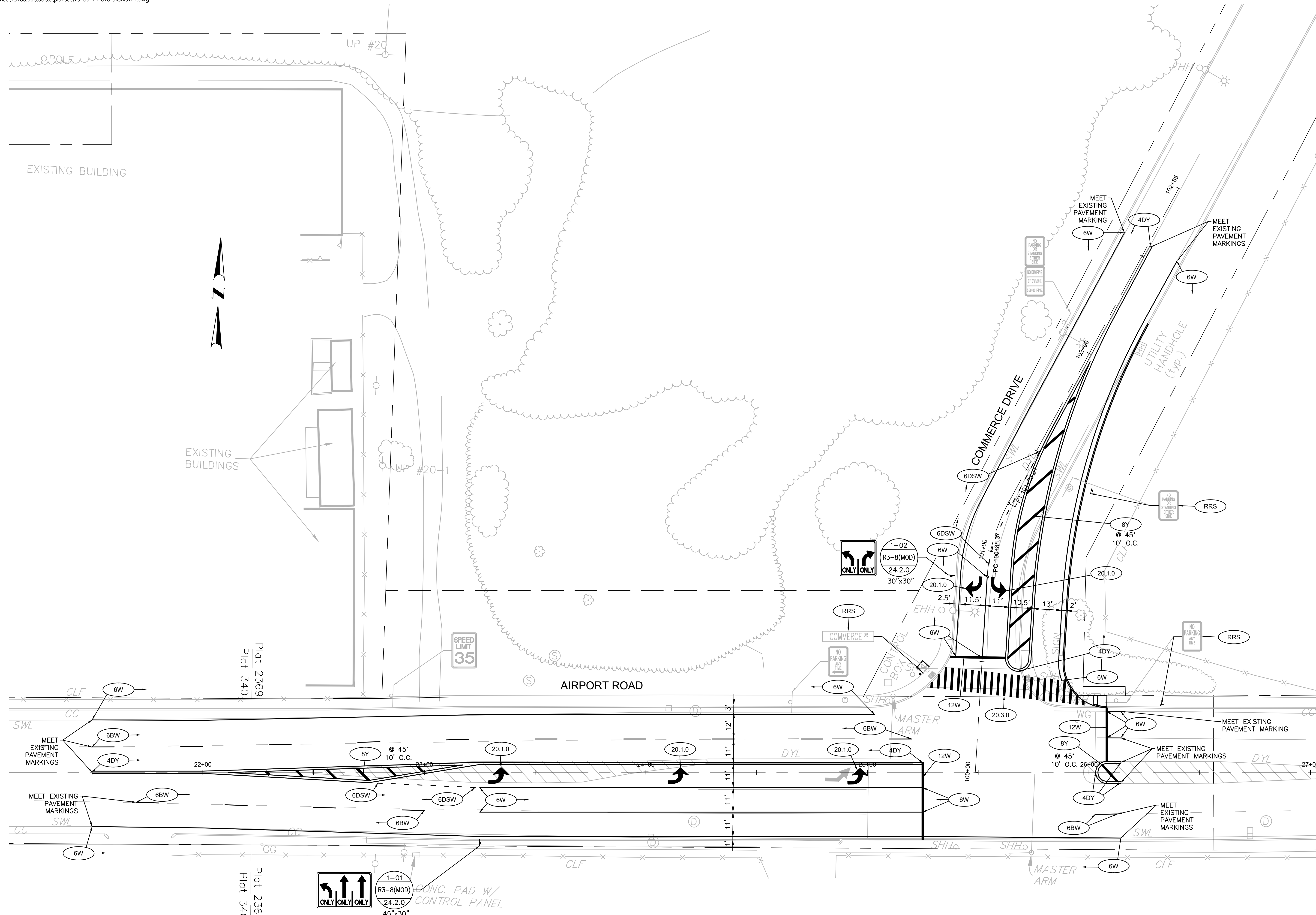
Designed by: _____ Checked by: _____
 Issued for: Construction Date: January 28, 2022

Location Plan

Saved Wednesday, January 26, 2022 2:33:03 PM TMELLO Plotted Wednesday, January 26, 2022 4:12:41 PM Tanya Niello



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NOTES:
1. CONTRACTOR TO COORDINATE WORK WITH COMMERCE DRIVE UTILITY IMPROVEMENTS AND PAVEMENT MILL AND OVERLAY PLANS.

Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island

| No. | Revision | Date | App'd. |
|-----|---------------------|-----------|--------|
| 1 | Final Plan Approval | 2/22/2022 | |

Designed by: _____ Checked by: _____
 Issued for: **Construction** Date: **January 28, 2022**

Signing & Striping Plan

Drawing Number

C-10

Sheet of 10 14

Project Number 73180.00

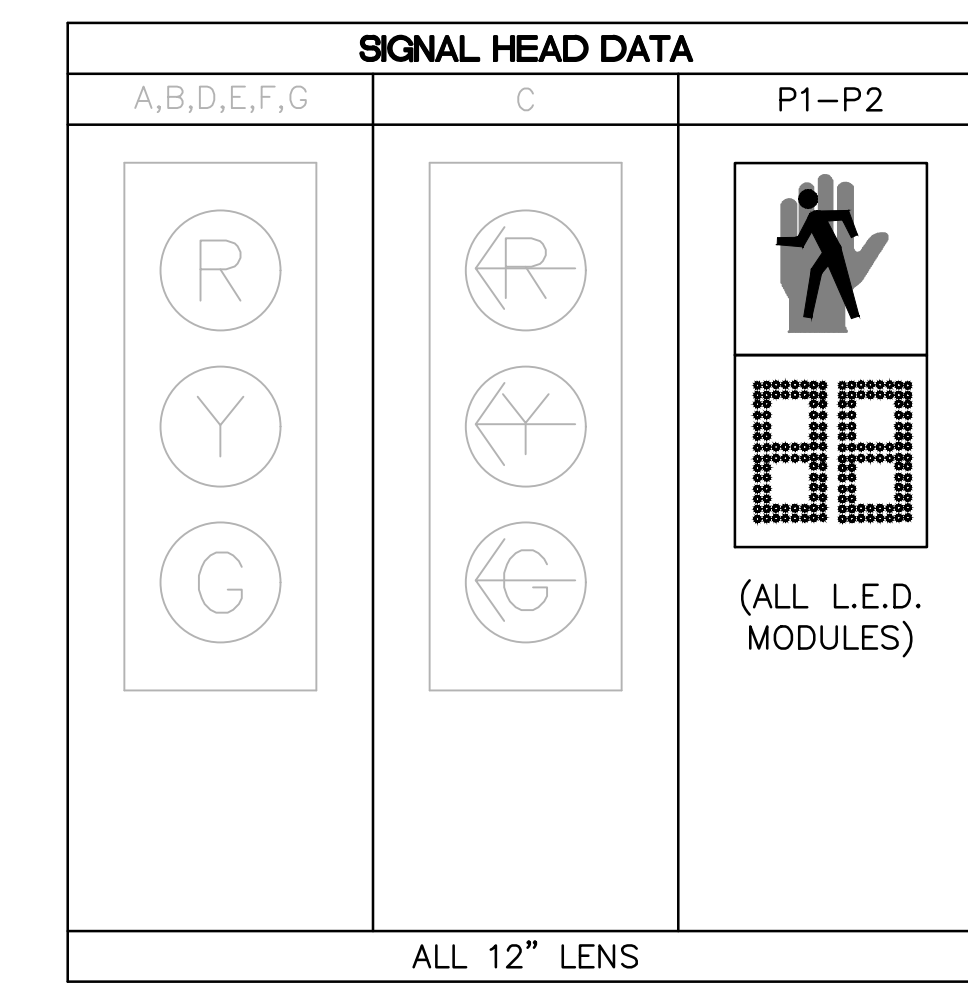
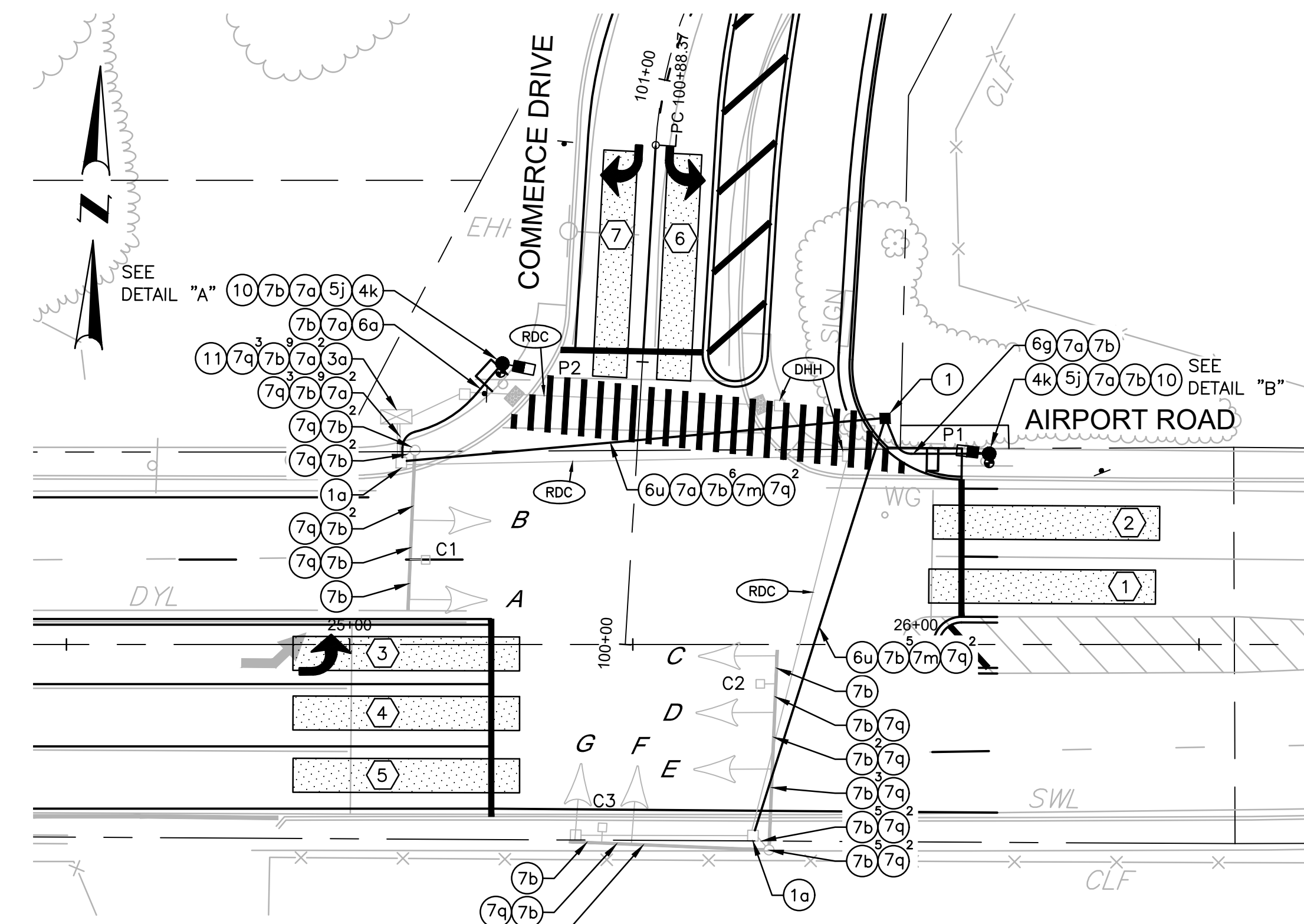
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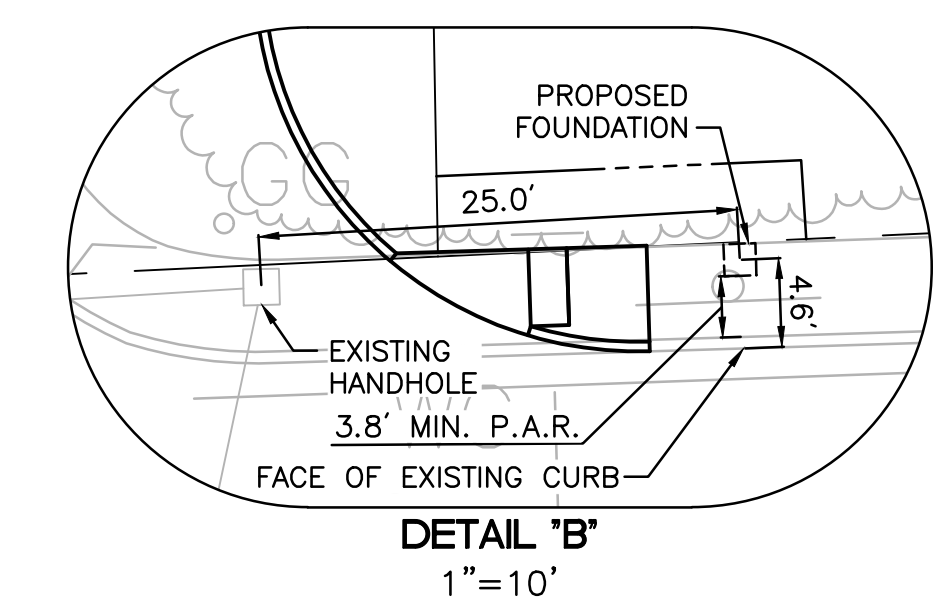
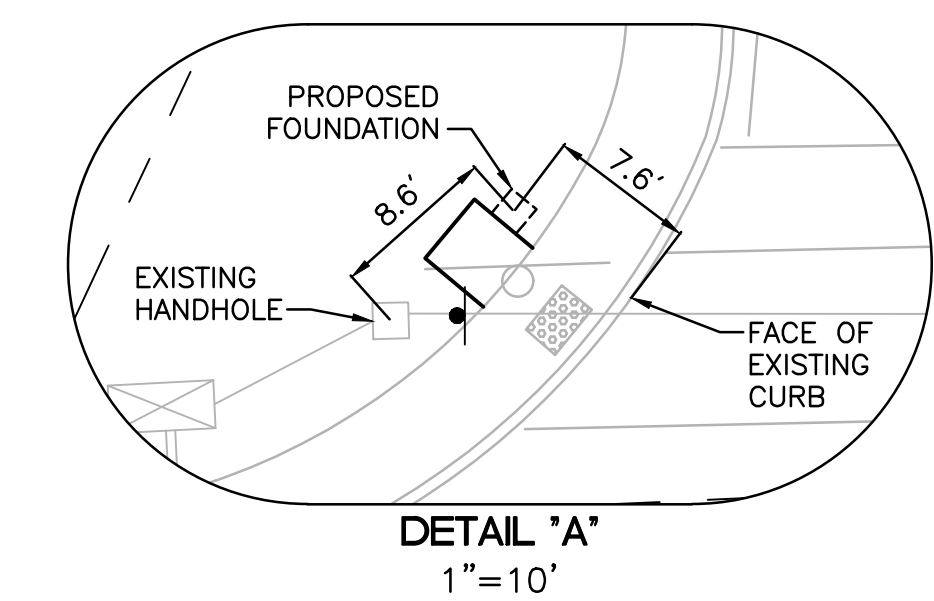


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401.272.8100

| ITEM NO. | ITEM CODE | ITEM DESCRIPTION |
|----------|-----------|---|
| 1 | T05.0100 | Precast Type "A" Handhole Std. 18.2.0 |
| 1a | T05.9901 | Break Into Existing Handhole |
| 3a | T12.9901 | Modify Existing Traffic Signal Controller Cabinet |
| 4k | T11.2008 | Traffic Signal Standard, 8 Foot, Std 19.4.0 Aluminum Pedestal Pole and Foundation |
| 5j | T14.9901 | 1 Way Pedestal Mounted LED Pedestrian Signal Head With Countdown Timer 12 Inch |
| 6a | T06.1020 | 2 Inch Rigid Steel Conduit - Underground |
| 6g | T06.3020 | 2 Inch Rigid Steel Conduit - Under Existing Pavement |
| 6u | T06.5430 | 3 Inch Schedule 80 Polyvinyl Chloride Plastic Conduit - Under Existing Pavement |
| 7a | T04.5303 | 14 AWG 3 Conductor Cable |
| 7b | T04.5305 | 14 AWG 5 Conductor Cable |
| 7m | T04.5001 | 6 AWG Single Conductor Cable 600v Insulation |
| 7q | T04.9901 | Thermal IP Video Detector Cable |
| 10 | T13.8210 | Accessible Pedestrian Detector - Pushbutton with Sign |
| 11 | 945.0100 | Remove and Dispose Traffic Signal Equipment |
| (DHH) | 201.0423 | Remove and Dispose Handhole |
| (RDC) | 201.0617 | Remove and Dispose Conduit - All Sizes |



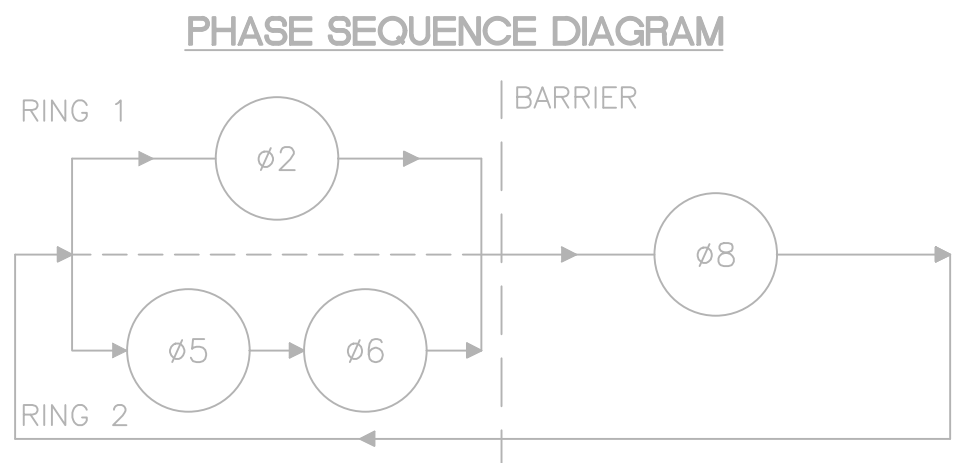
NOTES:
1. ALL TRAFFIC SIGNAL HEADS ARE EXISTING.
2. ALL PEDESTRIAN SIGNAL HEADS ARE PROPOSED.



NOTES:
1. CONTRACTOR TO COORDINATE WORK WITH COMMERCE DRIVE UTILITY IMPROVEMENTS AND PAVEMENT MILL AND OVERLAY PLANS.

TRAFFIC SIGNAL CONSTRUCTION NOTES:

- THE ITEM "REMOVE AND DISPOSE TRAFFIC SIGNAL EQUIPMENT" SHALL INCLUDE THE FOLLOWING MAJOR ITEMS:
MISCELLANEOUS TRAFFIC SIGNAL CABLE AND WIRING SHALL BE REMOVED AND LEGALLY DISPOSED OF IN ACCORDANCE WITH SECTION 945 OF THE STANDARD SPECIFICATIONS.
- THE EXISTING CONDUIT NETWORK SHOWN ON THIS PLAN IS BASED ON ASSUMED LOCATIONS AND SIZES. THE CONTRACTOR SHALL FIELD VERIFY THE EXISTING CONDUIT LOCATION AND SIZE FOR ACCURACY AND ADEQUACY PRIOR TO PERFORMING THE WORK.
- FINISHED GRADE OF PROPOSED TRAFFIC SIGNAL POLE FOUNDATIONS SHALL BE FLUSH WITH THE EXISTING OR PROPOSED FINISHED GRADE OF THE ADJACENT SIDEWALK. WHERE POLE FOUNDATIONS ARE PROPOSED WITHIN THE LIMITS OF WHEELCHAIR RAMPS, THE TOP OF FOUNDATION GRADE SHALL BE SET TO ALLOW THE POLE BASEPLATE AND/OR DECORATIVE BASE TO BE INSTALLED ABOVE FINISHED WHEELCHAIR RAMP GRADE.
- SEE JOB SPECIFIC PLAN SYMBOLS, LEGEND & NOTES PLAN FOR ADDITIONAL INFORMATION.
- PRIOR TO PERFORMING ANY WORK, THE CONTRACTOR SHALL CONTACT RIDOT (Russell Holt - russell.holt@dot.rhode.gov) AND HIGHWAY TECH OF MAINE (Bill@HighwayTech.com) TO COORDINATE ALL WORK.



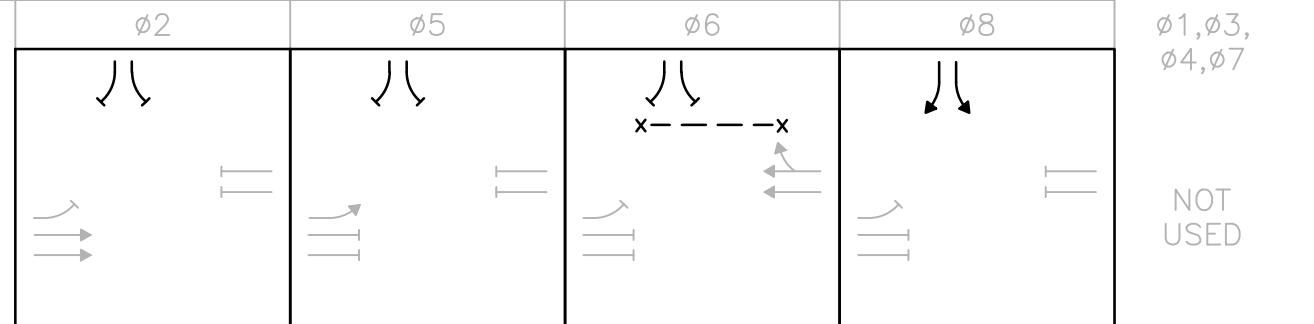
COORDINATION DATA (ALL ENTRIES IN SECONDS)

| | PLAN 1 | PLAN 2 | PLAN 3 | PLAN 4 | PLAN 5 |
|-------------------|--------|--------|--------|--------|--------|
| CYCLE LENGTH | 110 | 120 | 90 | 110 | 110 |
| OFFSET | 23 | 16 | 23 | 23 | 23 |
| SPLIT Ø2 | 85 | 96 | 65 | 95 | 95 |
| SPLIT Ø5 | 16 | 25 | 25 | 25 | 25 |
| SPLIT Ø6 | 69 | 71 | 40 | 70 | 70 |
| SPLIT Ø8 | 25 | 24 | 25 | 15 | 15 |
| COORDINATED PHASE | Ø6 | Ø6 | Ø6 | Ø6 | Ø6 |

| VIDEO DETECTOR DATA | | | | | |
|---------------------|---------------|------------------------|-------------|------------|----------|
| DETECTOR ZONE NO. | CAMERA NUMBER | APPROX. SIZE DET. ZONE | DELAY (SEC) | CALL PHASE | REMARKS |
| 1 | 1 | 6'x40' | 3 | Ø6 | PROPOSED |
| 2 | 1 | 6'x40' | 3 | Ø6 | PROPOSED |
| 3 | 2 | 6'x40' | 3 | Ø5 | PROPOSED |
| 4 | 2 | 6'x40' | 3 | Ø2 | PROPOSED |
| 5 | 2 | 6'x40' | 3 | Ø2 | PROPOSED |
| 6 | 3 | 6'x40' | 3 | Ø8 | PROPOSED |
| 7 | 3 | 6'x40' | 3 | Ø8 | PROPOSED |

| SEQUENCE AND TIMING DIAGRAM | | | | | | | | | | | | | | | | | | | |
|-----------------------------|-----------|---------|----------|-----|-----|-----|----------|-----|-----|------|----------|-----|-----|-----|----------|-----|-----|-----|--------------------|
| APPROACH | DIRECTION | HOUSING | Ø2 | | | | Ø5 | | | | Ø6 | | | | Ø8 | | | | FLASHING OPERATION |
| MINIMUM INTERVAL | | | 10 | | | | 5 | | | | 10 | | | | 5 | | | | |
| VEHICLE EXTENSION | | | 2.9 | | | | 2.6 | | | | 2.9 | | | | 2.6 | | | | |
| MAXIMUM 1 | | | 64 | | | | 21 | | | | 64 | | | | 22 | | | | |
| MAXIMUM 2 | | | 64 | | | | 21 | | | | 64 | | | | 22 | | | | |
| YELLOW CLEARANCE | | | | 4 | | | | 3.5 | | | | 4 | | | | 3.5 | | | |
| RED CLEARANCE | | | | | 1 | | | | 2.5 | | | | 3.5 | | | | 4 | | |
| PED. WALK/CHANGE | | | | | | | | | | 9/17 | | | | | | | | | |
| AIRPORT ROAD | WB | A,B | R | R | R | R | R | R | R | R | G | Y | R | R | R | R | R | R | FY |
| AIRPORT ROAD | EB-LT | C | (R) | (R) | (G) | (Y) | (R) | (R) | (R) | (R) | (R) | (R) | (R) | (R) | (R) | (R) | (R) | (R) | (FR) |
| AIRPORT ROAD | EB | D,E | G | Y | R | R | R | R | R | R | R | R | R | R | R | R | R | R | FY |
| COMMERCE DRIVE | SB | F,G | R | R | R | R | R | R | R | R | R | R | R | R | G | Y | R | R | FR |
| PEDESTRIAN X-ING | E-W | P1-P2 | DW | DW | DW | DW | DW | DW | DW | DW | W | DW | DW | DW | DW | DW | DW | DW | DARK |
| DETECTOR | | | NON-LOCK | | | | NON-LOCK | | | | NON-LOCK | | | | NON-LOCK | | | | |
| RECALL | | | MIN | | | | OFF | | | | MIN | | | | OFF | | | | |

SEQUENCE AND TIMING NOTES:
1. FLASHING OPERATION PER M.U.T.C.D.
2. MAXIMUM 2 = NOT USED
3. PED. W/FDW UPON PUSHBUTTON ACTUATION ONLY
4. THE PEDESTRIAN FDW INTERVAL SHALL NOT BE ACTIVE NOR DISPLAYED DURING THE YELLOW OR RED CLEARANCE INTERVALS



NOTES:
1. Ø6 "CALL NON ACTUATED" DURING COORDINATION.
2. OFFSET: BEG OF Ø6 YELLOW.
3. SPLIT TIMES EQUAL GREEN PLUS CLEARANCES.

NOTES:
1. DETECTION ZONE NOS. 1 - 6 ARE EXISTING ZONES THAT SHALL BE ADJUSTED TO THE PROPOSED LOCATIONS AS SHOWN ON THE PLAN.
2. DETECTION ZONE NO. 7 IS A NEW ZONE AND SHALL BE PROGRAMMED FOR THE PROPOSED LOCATION AS SHOWN ON THE PLAN.

Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island

| No. | Revision | Date | Appr'd. |
|-----|---------------------|-----------|---------|
| 1 | Final Plan Approval | 2/22/2022 | |

Designed by: _____ Checked by: _____
Issued for: _____ Date: _____
Construction January 28, 2022

Traffic Signal Plan

TRAFFIC CONTROL GENERAL NOTES:

- SEE RHODE ISLAND STANDARD 27.1.1 FOR SIGN DIMENSIONS AND PLACEMENT FOR WORK ZONE TRAFFIC FINES SIGN.
- CONE SPACING SHALL BE 10' O.C. WITHIN SHOULDER AND LANE TAPERS AND TRANSITION AREAS AND 20' O.C. WITHIN ALL OTHER AREAS, UNLESS OTHERWISE NOTED.
- ALL CONES SHALL CONFORM TO R.I. STANDARD 26.1.0.
- 11' MINIMUM LANE WIDTHS SHALL BE MAINTAINED UNLESS OTHERWISE NOTED ON THE PLANS.
- ALL MAINTENANCE AND PROTECTION OF TRAFFIC SETUPS SHALL BE IN CONFORMANCE WITH THE 2009 EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) AND RHODE ISLAND DEPARTMENT OF TRANSPORTATION GUIDELINES.
- ALL TRAFFIC CONTROL SETUPS SHALL BE COORDINATED WITH ADJACENT CONCURRENT CONSTRUCTION CONTRACTS TO DETERMINE IF SIGNS AND SETUPS SPECIFIED ARE APPROPRIATE.
- ALL TEMPORARY CONSTRUCTION SIGNS SHALL HAVE BLACK COPY ON ORANGE BACKGROUND AND SHALL BE INSTALLED ACCORDING TO THE RHODE ISLAND STANDARDS 24.3.0 & 29.1.0.
- ALL TRAFFIC CONTROL DEVICES, UNLESS OTHERWISE NOTED, SHALL CONFORM TO APPLICABLE SPECIFICATIONS OF THE M.U.T.C.D. PART 6, 2009 EDITION AND STANDARD HIGHWAY SIGNS, CURRENT EDITIONS. ALL SIGNS AND DRUMS SHALL BE REFLECTORIZED.
- EXISTING OR TEMPORARY SIGNS WHICH ARE IN CONFLICT WITH DAILY TRAFFIC MANAGEMENT SETUPS SHALL BE COVERED. IMMEDIATELY PRIOR TO COMPLETION, CONTRACTOR SHALL UNCOVER SIGNS AS DIRECTED BY THE ENGINEER. TEMPORARY CONSTRUCTION SIGNS INSTALLED BY THE CONTRACTOR SHALL BE REMOVED OR COVERED WHEN NOT IN USE.
- WORKERS SHALL WEAR RETROREFLECTIVE PERSONAL PROTECTIVE EQUIPMENT (PPE) IN ACCORDANCE WITH THE MUTCD AND FHWA REQUIREMENTS.
- ANY CONSTRUCTION SIGNS THAT ARE PLACED BEHIND CHANNELIZING DEVICES MUST BE MOUNTED AT A HEIGHT THAT WILL MAKE THEM VISIBLE TO MOTORISTS.
- SIGNS INSTALLED ON PORTABLE STANDS REQUIRE 12 INCH MINIMUM MOUNTING HEIGHT ABOVE THE ROADWAY SURFACE TO THE BOTTOM OF THE SIGN.
- TYPICAL LANE CLOSURE DETAILS DEPICT THE MINIMAL REQUIREMENTS FOR MAINTENANCE OF TRAFFIC. THE DETAILS SHALL BE USED AS A GUIDE TO PROVIDE TRAFFIC MANAGEMENT FOR DAILY OPERATIONS AND MAY BE MODIFIED AT THE DISCRETION OF RIDOT.
- POLICE DETAILS/FLAGGERS SHALL ASSIST WITH TRAFFIC CONTROL INCLUDING ASSISTING PEDESTRIANS AND ANY OVERSIZED VEHICLES TO SAFELY PASS THROUGH THE WORK ZONE, AS NEEDED.
- TO MINIMIZE THE IMPACTS TO TRAFFIC FLOW, THE CONTRACTOR SHALL LIMIT THE WORK AREA TO THE ACTUAL LIMIT OF WORK WITHIN THE ALLOWED WORK ZONES AND SHALL NOT TAKE THE ENTIRE WORK ZONE UNLESS IT IS REQUIRED FOR THE SPECIFIC ITEMS OF WORK BEING PERFORMED AND IS APPROVED BY THE ENGINEER.
- THE ENGINEER SHALL NOTIFY EACH ADJUTER APPROXIMATELY 48 TO 72 HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE THE TEMPORARY CLOSURE OR RESTRICTION OF ACCESS.
- SAFE ACCESS AND EGRESS TO ALL DRIVEWAYS AND STREETS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL SECURE WORK AREAS TO PREVENT UNAUTHORIZED ACCESS AT ALL TIMES.
- TEMPORARY CONSTRUCTION SIGNS AND BARRICADES SHALL BE IN PLACE PRIOR TO THE START OF WORK IN ANY AREA OPEN TO TRAFFIC.
- ALL TEMPORARY CONSTRUCTION SIGNS SHALL BE REMOVED OR COVERED WHEN NOT REQUIRED.
- AT NO TIME SHALL CONES, BARRICADES, OR DRUMS BE ALLOWED TO COVER PAVEMENT MARKINGS OR EXTEND INTO ANY OPEN TRAVEL LANE.
- POLICE OFFICER AHEAD W20-7(MOD) SIGNS SHALL BE UTILIZED IF A POLICE OFFICER IS ACTIVELY CONTROLLING TRAFFIC.
- AT NO TIME SHALL PERSONAL VEHICLES OR UNNECESSARY CONSTRUCTION EQUIPMENT BE LOCATED WITHIN THE RIDOT RIGHT-OF-WAY.
- CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING APPROPRIATE TRAFFIC CONTROL DEVICES AS NEEDED DURING CONSTRUCTION. ALL TRAFFIC CONTROL SHALL BE APPROVED BY THE ENGINEER.
- SYMBOLS SHOWN FOR SIGNS AND TYPE III BARRICADES ARE GRAPHICAL ONLY. WHERE SIDEWALKS ARE TO REMAIN OPEN, SIGNS AND BARRICADES SHALL BE LOCATED TO PROVIDE A MINIMUM CLEAR PEDESTRIAN PATH OF 48" (EXCLUDING CURB).
- TRUCK MOUNTED ATTENUATORS ON ROADWAYS WITH A POSTED SPEED OF 30 MPH OR LESS MAY BE USED AT CONTRACTOR'S OPTION. AT ALL OTHER LOCATIONS, TRUCK MOUNTED ATTENUATORS WILL BE REQUIRED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROVIDE CLEAR ZONES AROUND TRUCK MOUNTED IMPACT ATTENUATOR DEVICES AS REQUIRED BY THE MANUFACTURER.

TRAFFIC CONTROL PLANS SYMBOL LEGEND:

- TEMPORARY CONSTRUCTION SIGN. TYPE NOTED
- FLOURESCENT TRAFFIC CONE, R.I. STD. 26.1.0
- POLICE OFFICER WITH CRUISER & FLASHING LIGHTS
- FLAGGER
- TRUCK MOUNTED ATTENUATOR WITH FLASHING ARROW
- PORTABLE FLASHING ARROW
- WORK AREA
- TRAFFIC LANE DURING CONSTRUCTION

TAPER AND BUFFER LENGTHS

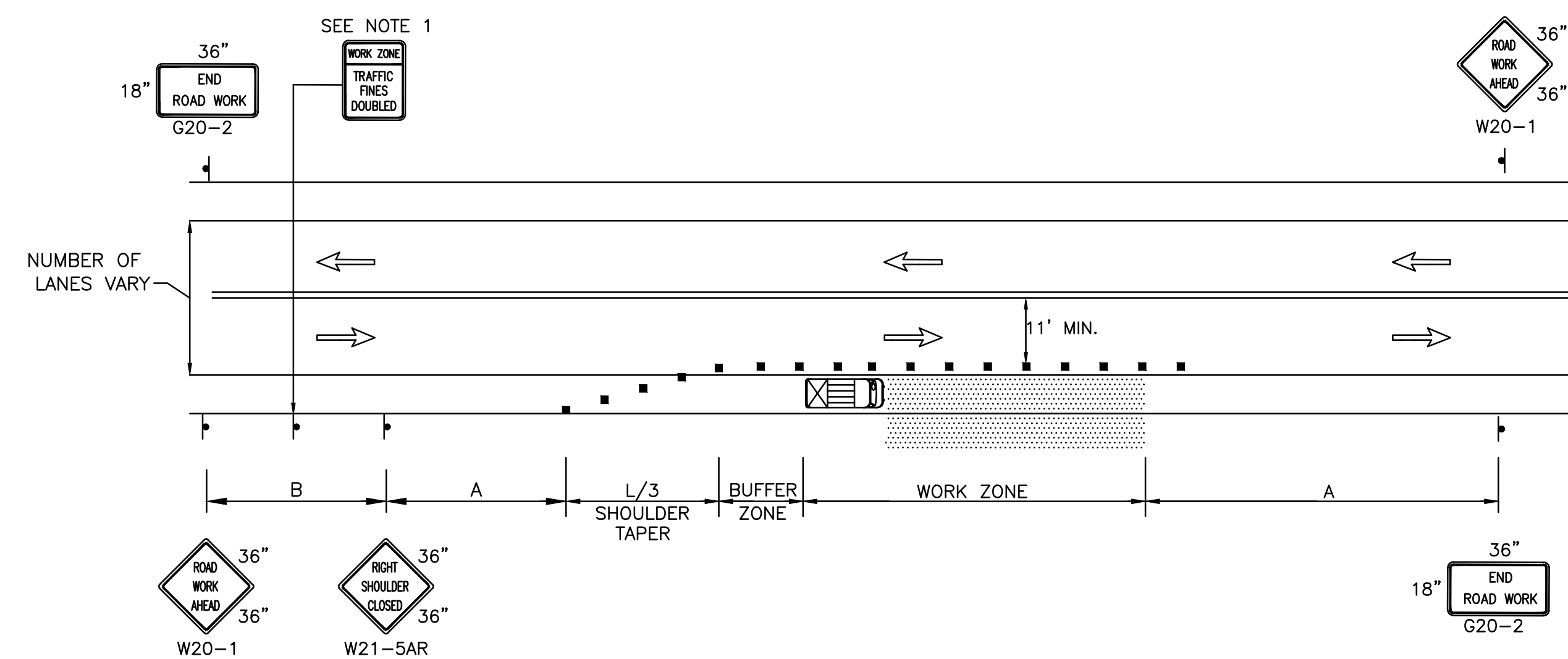
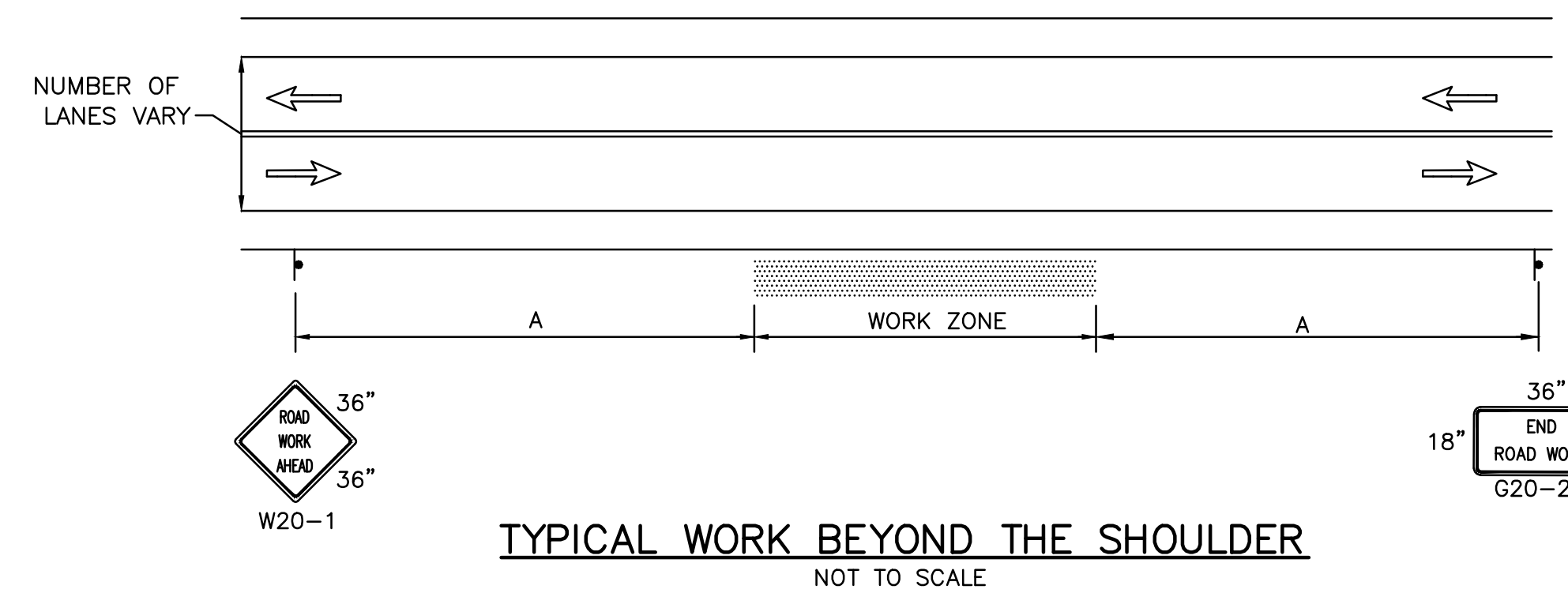
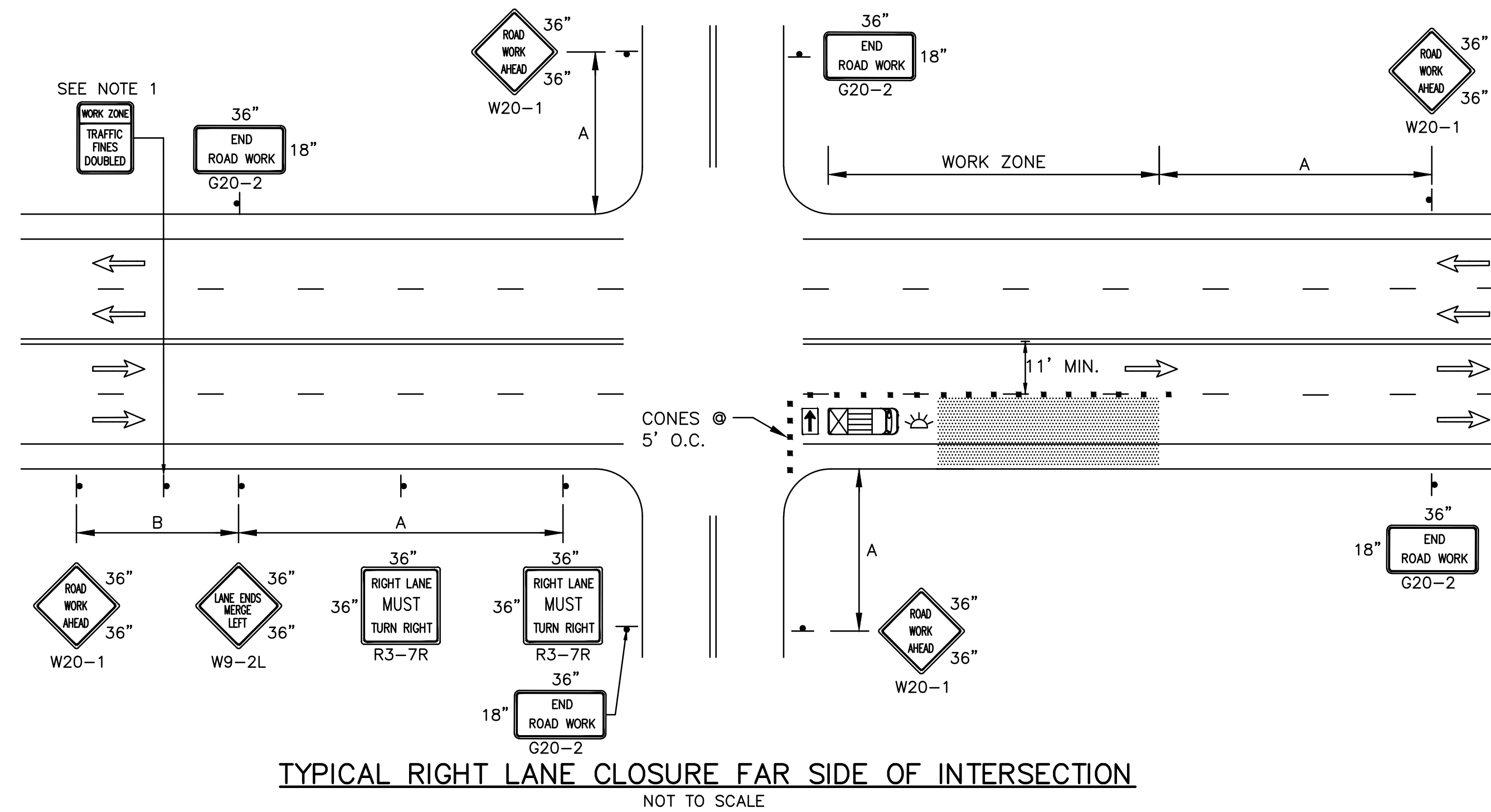
| SPEED LIMIT | TAPER LENGTH (L) FEET | BUFFER SPACE FEET |
|-------------|-----------------------|-------------------|
| 25 MPH | 125 | 55 |
| 30 MPH | 180 | 85 |
| 35 MPH | 245 | 120 |
| 40 MPH | 320 | 170 |
| 45 MPH | 540 | 220 |

MINIMUM ADVANCE WARNING SIGN SPACING

| | DISTANCE BETWEEN SIGNS (FEET) | | |
|------------------------------------|-------------------------------|-----|-----|
| | A | B | C |
| 30 MPH OR LESS URBAN OR RURAL AREA | 100 | 100 | 100 |
| 35 MPH OR GREATER URBAN AREA | 350 | 350 | 350 |

ROADWAY SEGMENT POSTED SPEED LIMIT

| POSTED SPEED LIMIT | ROADWAY SEGMENT | CITY |
|--------------------|-----------------|---------|
| 25 MPH | -COMMERCE DRIVE | WARWICK |
| 35 MPH | -AIRPORT ROAD | |



1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100

**Proposed
Airport Rd/
Commerce Drive
Intersection
Improvements**

Warwick, Rhode Island

| No. | Revision | Date | Appr'd. |
|-----|---------------------|-----------|---------|
| 1 | Final Plan Approval | 2/22/2022 | |

| Designed by | Checked by |
|--------------|------------------|
| Construction | January 28, 2022 |

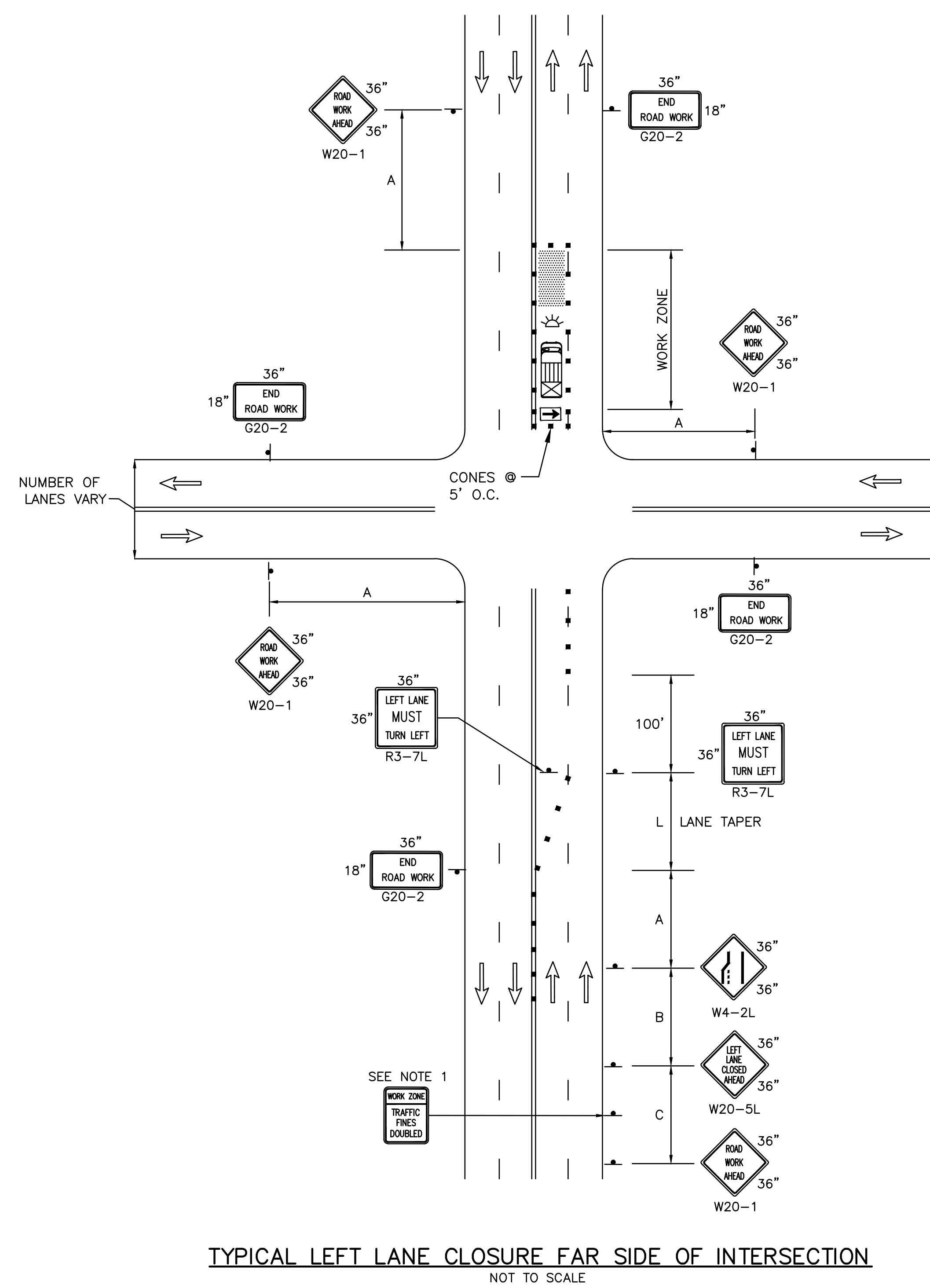
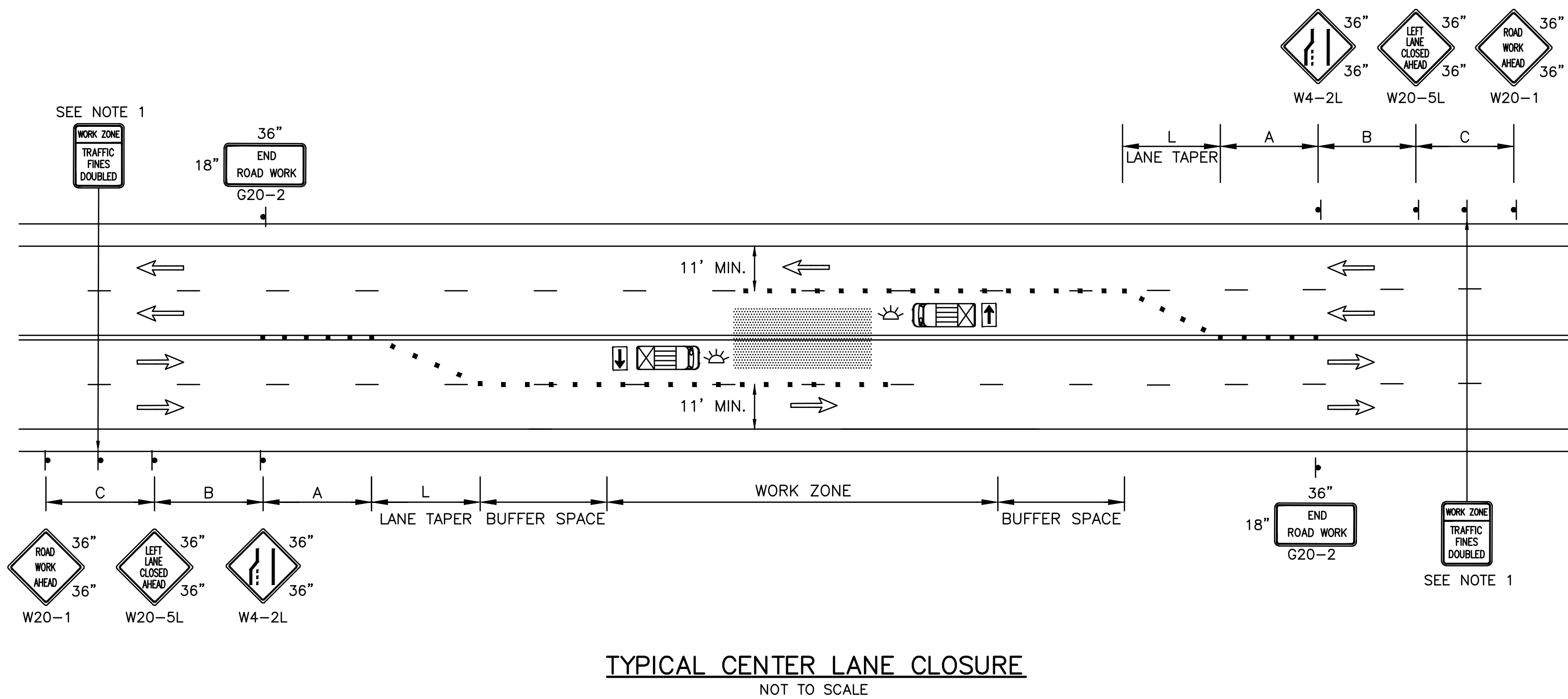
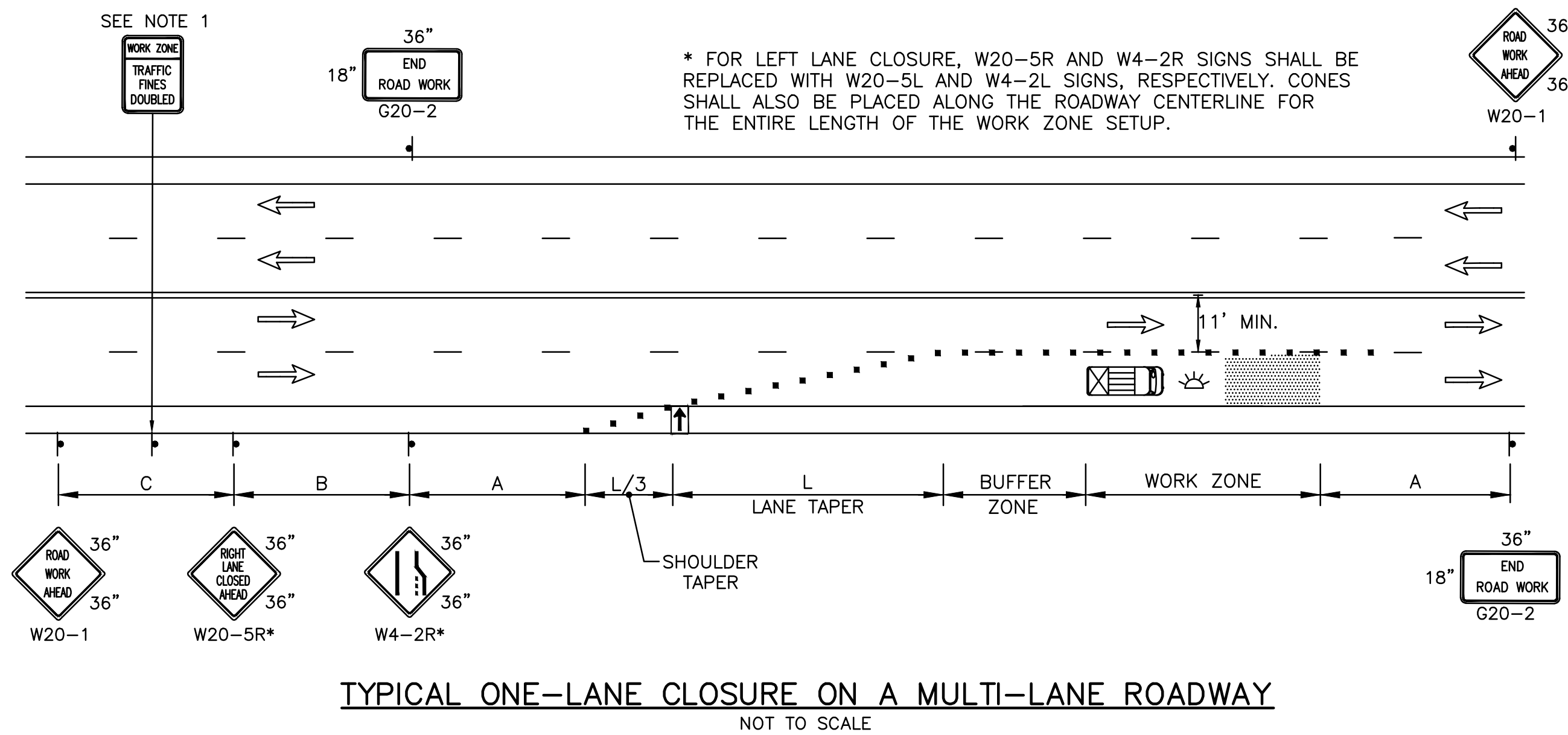
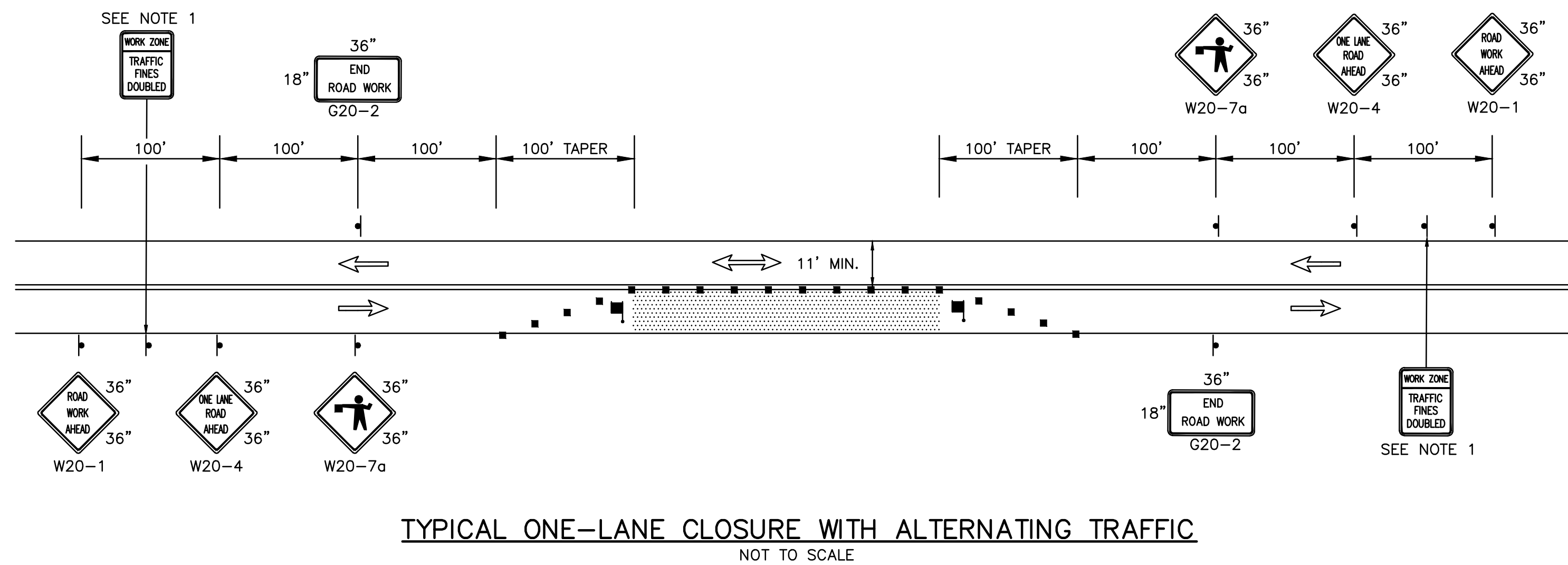
Traffic Control Plan 01

Drawing Number

C-12

Sheet of
12 of 14

Project Number
73180.00



**Proposed
Airport Rd/
Commerce Drive
Intersection
Improvements**

Warwick, Rhode Island

| No. | Revision | Date | Appvd. |
|-----|---------------------|-----------|--------|
| 1 | Final Plan Approval | 2/22/2022 | |

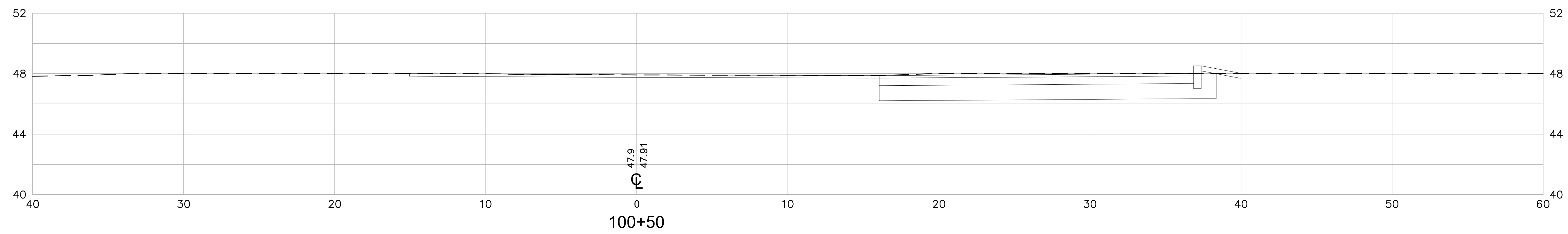
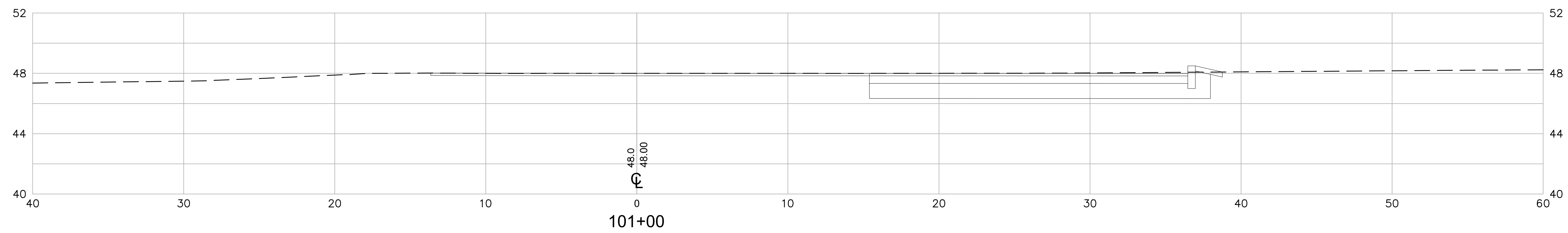
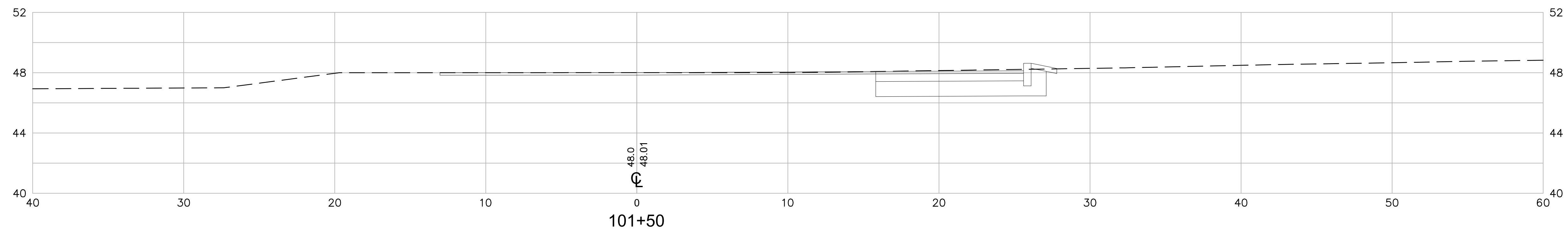
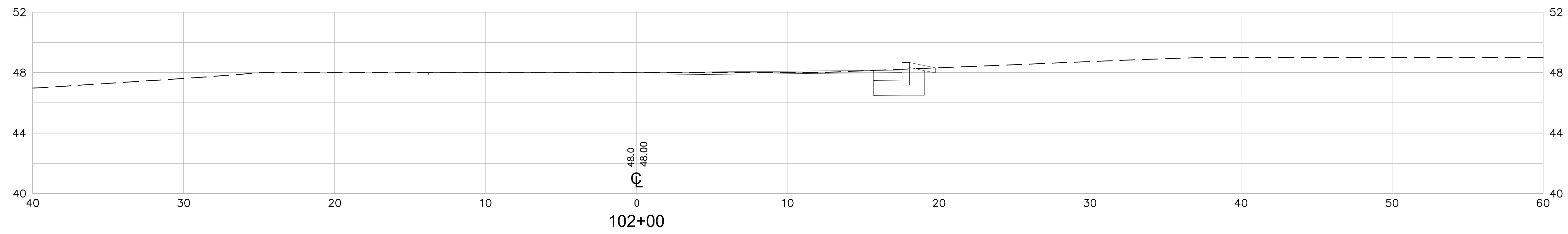
Designed by _____ Checked by _____
Issued for _____ Date _____

Construction January 28, 2022

Traffic Control Plan 02



1 Cedar Street
Suite 400
Providence, RI 02903
401.272.8100



Proposed Airport Rd/ Commerce Drive Intersection Improvements

Warwick, Rhode Island

| No. | Revision | Date | Appvd. |
|-----|---------------------|-----------|--------|
| 1 | Final Plan Approval | 2/22/2022 | |

| Designed by | Checked by |
|-------------|------------|
| | |

Issued for: **Construction** Date: **January 28, 2022**

Cross Sections

Drawing Number

C-14

Sheet **14** of **14**

Project Number
73180.00