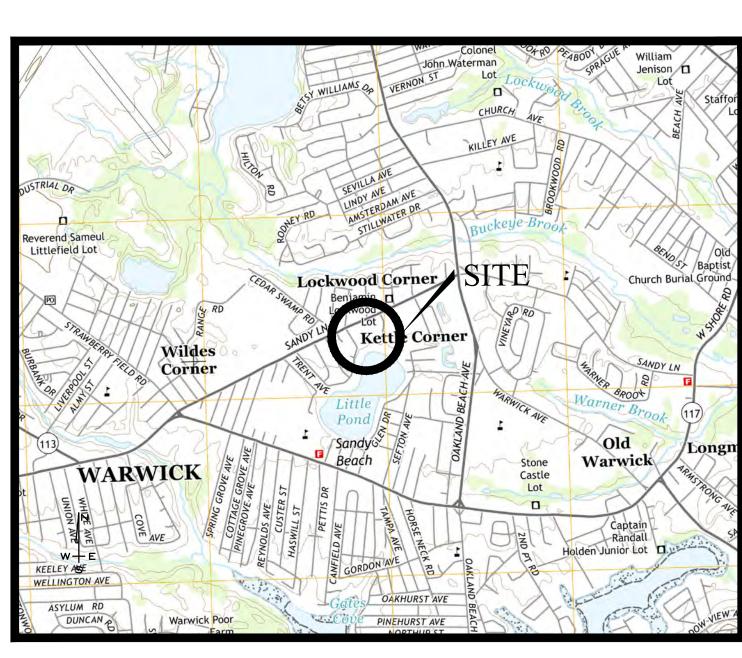
PRELIMINARY PLAN

FOR

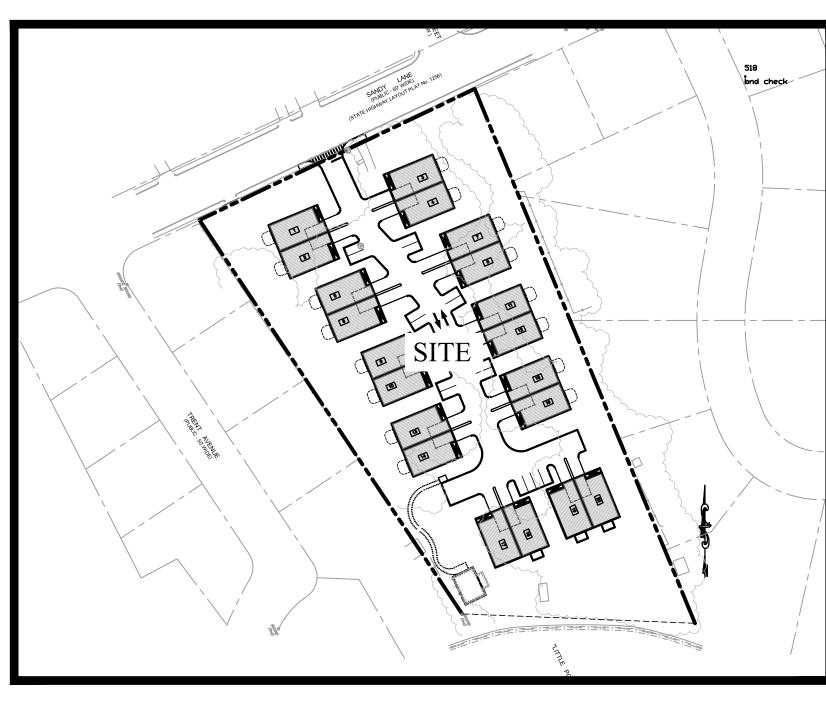
WATERSIDE AT LITTLE POND

A.P. 350, LOT 583 686 SANDY LANE WARWICK, RHODE ISLAND 02889



LOCUS MAP

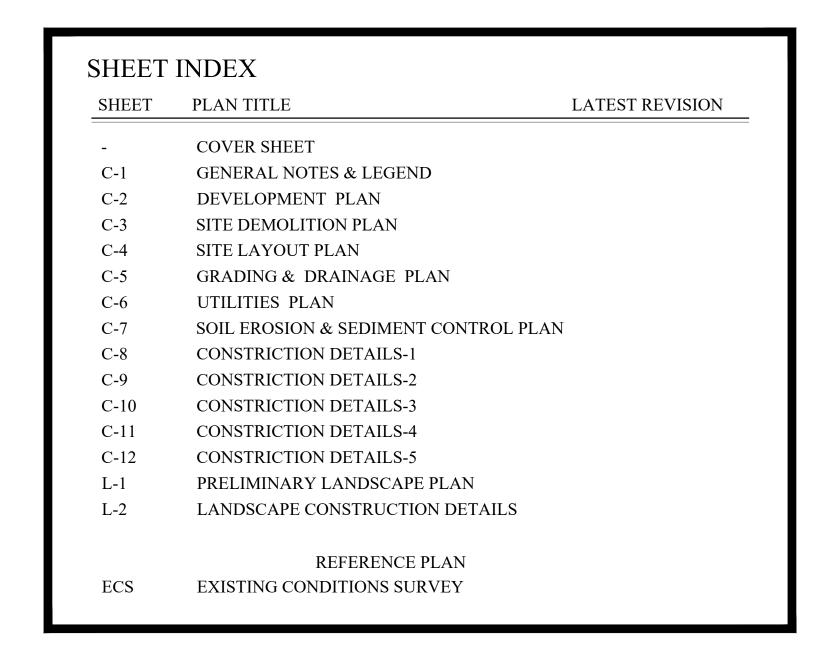
NOT TO SCALE



SITE PLAN

OWNER / APPLICANT:
ROBERT LAMOUREUX
CENTERVILLE BUILDERS
164 CENTERVILLE ROAD
WARWICK, RHODE ISLAND 02886

DATE: AUGUST, 2021 (REVISED: OCTOBER 25, 2021)



PREPARED BY:





JOB NO. 7247-00

GENERAL CONSTRUCTION NOTES

- AN APPROVED SET OF PLANS AND ALL APPLICABLE PERMITS MUST BE AVAILABLE AT THE CONSTRUCTION SITE TRAILER AT ALL TIMES. DEVIATIONS OR CHANGES WILL NOT BE ALLOWED UNLESS BY WRITTEN APPROVAL FROM THE ENGINEER.
- SITEWORK CONSTRUCTION SHALL NOT COMMENCE UNTIL ALL APPROVALS HAVE BEEN SECURED. REQUIRED PERMITS/APPROVALS FOR THE PROJECT INCLUDE BUT NOT LIMITED TO THE FOLLOWING: WARWICK MAJOR LAND DEVELOPMENT APPROVAL, RIDOT PHYSICAL ALTERNATION PERMIT, RIDEM STORMWATER CONSTRUCTION PERMIT AND WATER QUALITY CERTIFICATION, WARWICK WATER DEPARTMENT CONNECTION PERMIT & WARWICK SEWER AUTHORITY (WSA) CONNECTION PERMIT.
- ALL IMPROVEMENTS INDICATED HEREON MUST COMPLY WITH THE 'AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUILDELINES (ADAAG)'. NOTE THAT THE DETAIL CONTAINED WITHIN THIS PLAN MAY NOT SHOW THE DETAIL NECESSARY TO CONSTRUCT WALKWAYS. RAMPS AND SPACES TO COMPLY WITH THE ADAAG REQUIREMENTS BUT THE CONTRACTOR IS RESPONSIBLE TO PROVIDE THE LEVEL OF CARE NECESSARY TO BE CERTAIN THAT THE CONSTRUCTED PRODUCT MEETS THESE STANDARDS.
- THE CONTRACTOR MUST RETAIN THE SERVICES OF A REGISTERED LAND SURVEYOR IN THE STATE OF RHODE ISLAND TO LAYOUT ON THE GROUND ALL NEW ELEMENTS OF WORK. IF ANY WORK IS INSTALLED PRIOR TO THE ABOVE REQUIREMENT AND IF ANY WORK IS NOT SATISFACTORY TO THE ENGINEER, THE CONTRACTOR MUST REPLACE THE WORK AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR SHALL VERIFY THE PROPOSED LAYOUT WITH ITS RELATIONSHIP TO THE EXISTING SITE SURVEY. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS, SITE CONDITIONS AND MATERIAL SPECIFICATIONS AND SHALL NOTIFY THE OWNER AND ENGINEER OF ANY ERRORS, OMISSIONS OR DISCREPANCIES BEFORE COMMENCING, INSTALLING OR PROCEEDING WITH WORK.
- IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO DETERMINE THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES AND TO TAKE WHATEVER NECESSARY MEASURES NEEDED TO PROVIDE FOR THEIR PROTECTION. THE ENGINEER HAS DILIGENTLY ATTEMPTED TO LOCATE AND INDICATE ALL EXISTING UNDERGROUND UTILITIES AND FACILITIES ON THE DRAWINGS: HOWEVER. THE INFORMATION SHOWN IS FOR THE CONTRACTORS CONVENIENCE ONLY. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE LOCATIONS OF UTILITIES SHOWN OR NOT SHOWN. THE CONTRACTOR SHALL MAKE EXPLORATORY EXCAVATIONS AND LOCATE ANY EXISTING UTILITIES SUFFICIENTLY AHEAD OF CONSTRUCTION, VERIFY ALL DIMENSIONS, SITE CONDITIONS AND MATERIALS. THE CONTRACTOR MUST CONTACT THE LOCAL UTILITY COMPANIES FOR EXACT LOCATION OF UTILITIES PRIOR TO THE START OF ANY CONSTRUCTION AND SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES BEFORE START OF ANY WORK. IT IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO REPAIR AND REPLACE ANY AND ALL DAMAGE MADE TO UTILITIES BY THE CONTRACTOR.
- THE CONTRACTOR MUST NOTIFY OPERATORS WHO MAINTAIN UNDERGROUND UTILITIES IN THE AREA OF PROPOSED CONSTRUCTION, EXCAVATION OR BLASTING AT LEAST THREE WORKING DAYS, BUT NOT MORE THAN TEN WORKING DAYS PRIOR TO THE START OF ANY CONSTRUCTION, EXCAVATION OR BLASTING. ALL WATER, SEWER, GAS AND ALL OTHER UTILITIES MUST BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION.
- METHODS AND MATERIALS USED IN THE CONSTRUCTION OF IMPROVEMENTS SHALL CONFORM TO THE CURRENT CONSTRUCTION STANDARDS AND SPECIFICATIONS FOR THE CITY OF WARWICK AND THE STATE OF RHODE ISLAND DEPARTMENT OF TRANSPORTATION. THE STATE OF RHODE ISLAND STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2004 EDITION, AS AMENDED, AND THE RHODE ISLAND STANDARD DETAILS ARE MADE A PART HEREOF, AS IF ATTACHED HERETO.
- THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY PAVEMENT, DRIVEWAYS, SIDEWALKS, WALL, CURBS, ETC. DAMAGED DURING CONSTRUCTION WITH MATCHING MATERIALS.
- 10. THE CONTRACTOR AGREES THAT HE WILL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE PROJECT SITE CONDITIONS THROUGHOUT CONSTRUCTION. INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND THE ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED IN CONJUNCTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT.
- 11. ALL MATERIALS USED FOR CONSTRUCTION MUST BE NEW AND FREE OF DEFECTS. USED OR SALVAGED MATERIAL WILL NOT BE ALLOWED UNLESS WRITTEN APPROVAL FROM THE OWNER IS OBTAINED BY THE CONTRACTOR.
- 12. AT ALL TIMES THE CONTRACTOR MUST MAINTAIN ACCESS FOR EMERGENCY VEHICLES AROUND AND TO ALL BUILDINGS. (I.E. IN TIMES OF RAIN OR SNOW, ROADS MUST ABLE TO CARRY A FIRE TRUCK BY BEING PAVED OR HAVING A CRUSHED STONE BASE, ETC.). WIDTH OF EMERGENCY VEHICLE ACCESS MUST BE A MINIMUM OF 20 FEET WIDE. ACCESS TO BUILDINGS THAT HAVE A FIRE SPRINKLER SYSTEM OR STANDPIPE MUST BE WITHIN 40 FEET OF THE FIRE DEPARTMENT CONNECTION (FDC). NFPA 1141 3-1.
- 13. NECESSARY BARRICADES, LIGHTS, SIGNS AND OTHER TRAFFIC CONTROL METHODS AS MAYBE NECESSARY FOR THE PROTECTION AND SAFETY OF THE PUBLIC MUST BE PROVIDED AND MAINTAINED THROUGHOUT CONSTRUCTION BY THE CONTRACTOR.
- 14. HIGH INTENSITY LIGHTING FACILITIES MUST BE SO ARRANGED THAT THE SOURCE OF ANY LIGHT IS CONCEALED FROM PUBLIC VIEW AND FROM ADJACENT RESIDENTIAL PROPERTY AND DOES NOT INTERFERE WITH TRAFFIC.
- 15. ALL RI HIGHWAY BOUNDS AND PERMANENT SURVEY MARKERS SHALL BE PROTECTED THROUGHOUT CONSTRUCTION.
- 16. ALL TRAFFIC CONTROL SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES 2009 INCLUDING ALL REVISIONS.
- 17. REFER TO ARCHITECTURAL, STRUCTURAL, AND MECHANICAL PLANS FOR ALL BUILDING INFORMATION.
- 18. ALL CURB RADII ARE 5' UNLESS OTHERWISE NOTED ON THE SITE PLAN.
- 19. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR IS REQUIRED TO DEVELOP AND IMPLEMENT A PLAN FOR THE TEMPORARY CONTROL OF VEHICULAR AND PEDESTRIAN TRAFFIC FOR WORK WITHIN PUBLIC STREET RIGHT-OF-WAY AT THE SITE EGRESS. CONTRACTOR SHALL OBTAIN APPROVAL OF SAID PLAN FROM APPROPRIATE STATE AND COMMUNITY PUBLIC SAFETY OFFICIALS.
- 20. WHEN IT IS NECESSARY TO CLOSE OFF A STREET. THE FIRE DEPARTMENT AND POLICE DEPARTMENT SHALL BE NOTIFIED BY THE CONTRACTOR.
- 21. PRECAST STRUCTURES MAY BE USED AT CONTRACTOR'S OPTION. SHOP DRAWINGS OF PRECAST STRUCTURES SHALL BE REVIEWED BY THE ENGINEER AND APPROVED BEFORE USE.
- 22. IF ANY EXISTING STRUCTURES AND OR UTILITIES TO REMAIN ARE DAMAGED DURING CONSTRUCTION, EITHER ON THE PROJECT SITE, ADJACENT PROPERTIES, OR WITHIN STATE RIGHT-OF-WAY, IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER.

SURVEY REFERENCE

"EXISTING CONDITIONS SURVEY FOR PARCEL ID No. 350/0583, SITUATED ON 686 SANDY LANE, WARWICK, RHODE ISLAND, PREPARED FOR CENTERVILLE BUILDERS", PREPARED BY GAROFALO & ASSOCIATES, INC., DRAWING NO. 7247.00 ECS, DATED FEBRUARY, 2020.

GENERAL DRAINAGE & GRADING NOTES

- THE MAXIMUM RUNNING SLOPE ALONG ANY SIDEWALK SHALL BE 5%. THE MAXIMUM CROSS SLOPE ACROSS ANY SIDEWALK SHALL BE 2%. A MINIMUM 5'x5' LANDING SHALL BE PROVIDED IN FRONT OF ALL BUILDING ENTRANCES.
- ALL ADAAG PARKING SAPCES AND LOADING SPACES SHALL BE 2% MAXIMUM SLOPE IN ANY DIRECTION.
- CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
- ALL GRATES AND COVERS IN PAVED AREAS SHALL BE FLUSH WITH PAVEMENT, AND SHALL HAVE TRAFFIC BEARING RING & COVERS. MANHOLES IN UNPAVED AREAS SHALL BE 3" ABOVE FINISH GRADE.
- CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
- ALL UNSURFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL. CONTRACTOR SHALL APPLY STABILIZATION FABRIC TO ALL SLOPES 3H:1V OR STEEPER. CONTRACTOR SHALL GRASS DISTURBED AREAS IN ACCORDANCE WITH THE CITY OF PROVIDENCE SPECIFICATIONS UNTIL A HEALTHY STAND OF GRASS IS OBTAINED.

GENERAL UTILITY NOTES:

- THE CONTRACTOR SHALL NOTIFY DIG-SAFE (1-888-344-7233) AND ALL LOCAL AUTHORITIES & UTILITY COMPANIES TO VERIFY LOCATIONS OF UTILITIES WITHIN THE AREA 72 HOURS PRIOR TO BEGINNING ANY EXCAVATION OR DEMOLITION FOR THE PURPOSE OF COORDINATING THE MARKING OF UNDERGROUND UTILITIES. LOCATION AND DEPTHS OF EXISTING UTILITIES SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY, LOCATE AND PROTECT EXISTING UTILITIES IN THE FIELD WHETHER OR NOT SHOWN ON THE DRAWINGS
- 2. ALL WORK SHALL BE IN COMPLETE ACCORDANCE WITH ALL APPLICABLE STATE, FEDERAL AND LOCAL CODES. AND ALL NECESSARY LICENSES AND PERMITS SHALL BE OBTAINED BY THE CONTRACTOR AT HIS EXPENSE UNLESS PREVIOUSLY OBTAINED BY THE OWNER/DEVELOPER. THE CONTRACTOR SHALL REFER TO THE WARWICK WATER DEPARTMENT RULES & REGULATIONS FOR THE SERVICE INSTALLATION & EXTENSION REQUIREMENTS OF WATER LINES.
- 3. THE CONTRACTOR SHALL COORDINATE LOCATION AND INSTALLATION OF ALL UNDERGROUND UTILITIES AND APPURTENANCES TO MINIMIZE DISTURBANCE OF CURBING, PAVING AND COMPACTED SUBGRADE, THE CONTRACTOR SHALL NOTIFY THE TOWN ENGINEER & ALL LOCAL UTILITY COMPANIES 48 HOURS BEFORE EACH PHASE OF CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY INFORM THE ENGINEER OF ANY DISCREPANCIES OR ERRORS DISCOVERED IN THE PLANS.
- BEDDING REQUIREMENTS SPECIFIED HEREIN ARE TO BE CONSIDERED AS MINIMUMS FOR RELATIVELY DRY, STABLE EARTH CONDITIONS. ADDITIONAL BEDDING SHALL BE REQUIRED FOR ROCK TRENCHES AND WET AREA. CONTRACTOR SHALL HAVE THE RESPONSIBILITY TO PROVIDE SUCH ADDITIONAL BEDDING AS MAY BE REQUIRED TO PROPERLY CONSTRUCT THE WORK.
- 5. THE CONTRACTOR SHALL REMOVE ANY ABANDONED FOUNDATIONS, UTILITY STRUCTURES, BURIED DEBRIS ETC. WHICH INTERFERE WITH THE INSTALLATION OF THE UTILITY WORK. ALL SUCH STRUCTURES SHALL BE COMPLETELY REMOVED AND THE EXCAVATED AREA SHALL BE BACKFILLED WITH COMPACTED GRAVEL IN 6" LIFTS TO 95% COMPACTION TO 6" BELOW THE BOTTOM OF THE UTILITY AND PIPE.
- 6. COMPACTION OF THE BACKFILL OF ALL TRENCHES SHALL BE COMPACTED TO THE DENSITY OF 95% OF THE THEORETICAL MAXIMUM DRY DENSITY (ASTM D698). BACKFILL MATERIAL SHALL BE FREE FROM ROOTS. STUMPS OR OTHER FOREIGN DEBRIS AND SHALL BE PLACED IN LIFTS NOT TO EXCEED ONE FOOT IN COMPACTED FILL THICKNESS. CORRECTION OF ANY TRENCH SETTLEMENT WITHIN A YEAR FROM THE DATE OF PROJECT APPROVAL WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- 7. ALL PIPING LAYOUT INDICATED ON THESE PLANS IS DIAGRAMMATIC ONLY AND DOES NOT SHOW ALL THE REQUIRED FITTINGS FOR PROPER ALIGNMENT. THE CONTRACTOR SHALL PROVIDE ALL REQUIRED FITTINGS TO OBTAIN PROPER ALIGNMENT AND FOR EXISTING UTILITY CONNECTIONS BASED UPON FIELD CONDITIONS.
- 8. IF DURING EXCAVATION THE TRENCH WIDTH EXCEEDS THE SUM OF THE PIPE O.D. PLUS 2'-0". PLACE AND COMPACT THE FILL TO 12" ABOVE THE PIPE AND RE-EXCAVATE TO REQUIRED GRADE.
- 9. DOMESTIC AND FIRE PROTECTION WATER SERVICE IS PROVIDED TO THE PROPERTY BY THE WARWICK WATER DEPARTMENT. THE ONSITE WATER SYSTEM ON THE PROPERTY IS PRIVATE WATER PIPING AND SHALL BE (AWWA C901) DR9 POLYETHYLENE OR TYPE k COPPER FOR 2" SERVICES OR SMALLER, OR CLASS 52 DOUBLE CEMENT LINED DUCTILE IRON PIPE WITH TYTON JOINTS FOR 4" SERVICES OR LARGER. ALL BENDS, TEES ETC. SHALL BE JOINT RESTRAINED BY THE USE OF CONCRETE THRUST BLOCKS AND "MEG-A-LUG". ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE WARWICK WATER DEPARTMENT REGULATIONS, STANDARDS AND SPECIFICATIONS. NOTIFICATION SHALL BE PROVIDED TO SAID AUTHORITIES AT LEAST 72 HOURS PRIOR TO INITIATING CONSTRUCTION.
- 10. GATE VALVES SHALL BE CAST IRON BODY BRONZE MOUNTED, COMPLETE WITH ROAD BOX AND SHALL CONFORM TO THE WARWICK WATER DEPARTMENT REGULATIONS, STANDARDS AND SPECIFICATIONS.
- 11. ALL FIRE AND PLUMBING FIXTURES MUST CONFORM TO LOCAL SPECIFICATIONS AND AS STIPULATED BY THE LOCAL FIRE MARSHALL AND/OR THE BUILDING OFFICIAL.
- 12. ALL SANITARY SEWER MATERIALS AND WORKMANSHIP ASSOCIATED WITH THE INSTALLATION AND TESTING OF SANITARY SEWERS SHALL CONFORM TO THE WARWICK SEWER AUTHORITY (WSA) REGULATIONS. STANDARDS AND SPECIFICATIONS. ALL SEWER SYSTEM COMPONENTS MUST BE TESTED. CLEANED AND AS-BUILT INFORMATION MUST BE SUBMITTED TO THE CITY OF WARWICK AND APPROVED, PRIOR TO ISSUANCE OF A PERMIT TO CONNECT TO PUBLIC SEWER SYSTEM.
- 13. SANITARY SEWER MAINS AND SERVICES SHALL BE SDR-35 (ASTM D-3034) PVC SEWER PIPE WITH PUSH ON RUBBER RING JOINTS UNLESS NOTED OTHERWISE. CLEANOUTS SHALL BE INSTALLED AT BUILDING FACE (REFER PLUMBING PLANS FOR ALL WORK WITHIN 5' OF THE BUILDING). NEOPRENE COUPLINGS WITH STAINLESS STEEL BAND AND SHEER RINGS SHALL BE REQUIRED FOR JOINING DIFFERENT TYPES OF SANITARY SEWER PIPES.
- 14. SEWER LINES SHALL BE INSTALLED AT A MINIMUM 10 FOOT HORIZONTAL SEPARATION FROM ANY PROPOSED OR EXISTING WATER LINE.
- 15. WHENEVER SEWER LINES MUST CROSS WATER LINES, THE SEWER SHALL BE INSTALLED SO THAT THE TOP OF THE SEWER IS AT LEAST 18 INCHES BELOW THE BOTTOM OF THE WATER MAIN. WHERE 18 INCH VERTICAL SEPARATION & 10 FOOT HORIZONTAL SEPARATION CAN NOT BE MET AT WATER AND SEWER CROSSINGS. THE SEWER PIPE SHALL BE ENCASED IN CONCRETE OR BE EITHER DUCTILE IRON OR C900 BLUE BRUTE FOR A DISTANCE OF 10 FEET ON EACH SIDE OF CROSSING.
- 16. WHENEVER NEW SEWER LINES CONNECT TO EXISTING SEWER MANHOLES THE CONTRACTOR SHALL REBUILD THE EXISTING SEWER MANHOLE CHANNEL TO ACCOMMODATE THE NEW CONNECTION.
- 17. STORM DRAINS 12" AND OVER SHALL BE SMOOTH INTERIOR WALL AND EXTERIOR CORRUGATED HIGH DENSITY POLYETHYLENE (HDPE) PIPE CAPABLE OF WITHSTANDING (H-20) LOAD UNLESS NOTED OTHERWISE. PIPE SHALL BE JOINED USING BELL & SPIGOT JOINTS MEETING OR EXCEED ASTM F2648. THE JOINT SHALL BE SOIL-TIGHT AND GASKETS SHALL MEET OR EXCEED ASTM F477. HDPE PIPE SHALL BE AS MANUFACTURED BY ADVANCED DRAINAGE SYSTEMS INC. (ADS), HANCOR PIPE OR LANE PIPE. ALL STORM DRAINAGE PIPING SHALL BE LAID ON A SMOOTH CONTINUOUS GRADE WITH NO VISIBLE BENDS AT THE JOINTS. WHERE INDICATED ON DRAWINGS REINFORCED CONCRETE PIPE (RCP) PIPE SHALL BE CLASS III RCP WITH "O" RING GASKET JOINTS
- 18. GAS SERVICE FACILITIES SHALL BE DESIGNED BY OTHERS. CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE UTILITY INSTALLATIONS AS REQUIRED TO ENSURE ADEQUATE GAS SERVICE IS PROVIDED AND SHALL BE RESPONSIBLE FOR ALL INSTALLATION PROCEDURES (TRENCHING, LAYING PIPE, ETC.) AS ARE REQUIRED BY THE GAS COMPANY FOR COMPLETE AND IN PLACE CONSTRUCTION.

- 19. ELECTRIC SERVICE FACILITIES SHALL BE DESIGNED BY OTHERS. ELECTRIC SERVICE AND TRANSFORMER PAD SHALL CONFORM TO THE REQUIREMENTS OF THE ELECTRIC COMPANY. CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING ADEQUATE ELECTRIC SERVICE IS PROVIDED AND SHALL BE RESPONSIBLE FOR ALL INSTALLATION PROCEDURES (TRENCHING, LAYING PIPE, ETC.) AS ARE REQUIRED BY THE ELECTRIC COMPANY FOR COMPLETE AND IN PLACE CONSTRUCTION. REFER TO ELECTRICAL DRAWINGS FOR DETAILS ON ALL UNDERGROUND ELECTRIC.
- 20. TEL/CABLE SERVICE FACILITIES SHALL BE DESIGNED BY OTHERS. CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING ADEQUATE UTILITY SERVICE IS PROVIDED AND SHALL BE RESPONSIBLE FOR ALL INSTALLATION PROCEDURES (TRENCHING, LAYING PIPE ETC.) AS IS REQUIRED BY THE LOCAL UTILITY CO. FOR COMPLETE AND IN PLACE CONSTRUCTION. REFER TO ELECTRICAL DRAWINGS FOR ALL UNDERGROUND ELECTRIC.
- 23. SITE LIGHTING ELEMENTS ARE SHOWN FOR INFORMATIONAL PURPOSES ONLY. THE CONTRACTOR SHALL PROVIDE A PHOTOMETRICS PLAN TO THE CITY OF WARWICK PRIOR TO CONSTRUCTION. THE PLAN SHALL MEET THE APPLICABLE REQUIREMENTS OF THE CITY OF WARWICK. SITE CONTRACTOR SHALL BE RESPONSIBLE TO COORDINATE INSTALLATION OF LIGHT POLE BASES IN LOCATIONS INDICATED. REFER TO SITE LIGHTING PLANS FOR INSTALLATION REQUIREMENTS.
- 24. WHENEVER UTILITIES ARE TO BE INSTALLED WITHIN THE SANDY LANE RIGHT OF WAY, THE TRENCH MUST BE BACKFILLED WITH FLOWABLE FILL. ALL AREAS OF ROADWAY PAVEMENT DISTURBED DURING CONSTRUCTION SHALL BE RE-PAVED PER THE CITY OF WARWICK AND STATE STANDARDS AND SPECIFICATIONS.

STORMWATER SYSTEM MAINTENANCE NOTES

THE DRAINAGE SYSTEMS ARE TO BE MONITORED THROUGHOUT THE ENTIRE CONSTRUCTION PERIOD BY THE CONTRACTOR. UPON COMPLETION OF THE PROJECT THE CONTRACTOR MUST DO A FINAL FULL MAINTENANCE & CLEAN UP OF THE STORMWATER MANAGEMENT SYSTEM AND THE SITE. UPON COMPLETION OF THE CONTRACTOR'S FINAL MAINTENANCE & CLEAN UP OF THE PROJECT, MONITORING OF THE STORMWATER MANAGEMENT SYSTEM SHALL BE THE RESPONSIBILITY OF THE OWNER.

CONSTRUCTION MONITORING/MAINTENANCE PROCEDURES SHALL BE AS FOLLOWS: (RESPONSIBILITY OF CONTRACTOR)

1. SILT BARRIER:

MONITOR SILT BARRIER ON A WEEKLY BASIS AND AFTER EVERY RAIN STORM EVENT AND REPAIR OR REPLACE ANY DAMAGED AREAS IMMEDIATELY. IMMEDIATELY CLEAN THE SILT BARRIER IF SIX INCHES OR MORE OF SEDIMENT HAS ACCUMULATED ON THE HAYBALE & SILT BARRIER.

- 2. PAVED AREAS: PARKING LOTS. PUBLIC & PRIVATE ROADWAYS AND GUTTERS SHALL BE SWEPT CLEAN OF ALL SEDIMENT & DEBRIS. SWEEPING & REMOVAL OF DEBRIS SHALL BE PERFORMED ON A WEEKLY BASIS AT A MINIMUM.
- 3. CATCH BASINS: ALL CATCH BASINS SHALL BE INSTALLED AS DETAILED AND INSPECTED AFTER EVERY RAIN STORM EVENT IMMEDIATELY CLEAN THE CATCH BASIN SUMP IF TWO FEET OR MORE OF SEDIMENT HAS ACCUMULATED WITHIN THE CATCH BASIN.
- 4. DRAIN MANHOLES: DRAIN MANHOLES SHALL BE INSTALLED AS DETAILED AND INSPECTED AFTER EVERY RAIN STORM EVENT IMMEDIATELY CLEAN THE DRAIN MANHOLE IF ONE FOOT OR MORE OF SEDIMENT HAS ACCUMULATED WITHIN THE DRAIN MANHOLE.
- 5. STORMWATER BMPS: NO CONSRUCTION RUNOFF SHALL BE DIRECTED TO STORMWATER BMPS UNTIL UPGRADIENT AREAS ARE

POST CONSTRUCTION MONITORING/MAINTENANCE PROCEDURES SHALL BE AS FOLLOWS (RESPONSIBILITY OF OWNER)

PARKING LOTS, ROADS AND ALL ACCESS WAYS AND GUTTERS MUST BE SWEPT CLEAN OF ALL SEDIMENT AND DEBRIS ON BI-ANNUAL BASIS IN SPRING AND FALL OF EACH YEAR OR AS NECESSARY

ALL CATCH BASINS MUST BE INSPECTED AND MAINTAINED ON A BI-ANNUAL BASIS IN MARCH AND OCTOBER OF EACH YEAR. CATCH BASINS MUST BE INSPECTED TO ENSURE THEY HAVE ADEQUATE SUMP CAPACITY FRAMES AND GRATES ARE NOT DAMAGED, AND OIL/WATER SEPARATING DEVICES ARE IN PLACE. CATCH BASIN SUMPS ARE TO BE CLEANED OUT DURING BI-ANNUAL INSPECTIONS IN MARCH AND OCTOBER OF EACH YEAR. IMMEDIATELY CLEAN THE CATCH BASIN SUMP IF TWO FEET OR MORE OF SEDIMENT HAS ACCUMULATED WITHIN THE CATCH BASIN.

3. DRAIN MANHOLES: ALL DRAIN MANHOLES MUST BE INSPECTED AND MAINTAINED ON A BI-ANNUAL BASIS IN MARCH AND OCTOBER OF EACH YEAR. DRAIN MANHOLES MUST BE INSPECTED TO ENSURE FRAMES AND COVERS ARE NOT DAMAGED AND NO BLOCKAGES HAVE OCCURRED WITHIN THE MANHOLE. DRAIN MANHOLES ARE TO BE CLEANED OUT DURING BI-ANNUAL INSPECTIONS IN MARCH AND OCTOBER OF EACH YEAR. IMMEDIATELY CLEAN THE DRAIN MANHOLE IF ONE FOOT OR MORE OF SEDIMENT HAS ACCUMULATED.

4. STORMWATER MANAGEMENT FACILITIES - REFER TO OPERATIONS AND MAINTENANCE PLAN UNDER SEPARATE COVER. (NOTE: A LEGALLY ENFORCEABLE MAINTENANCE AGREEMENT SHALL BE EXECUTED IF THE PROPERTY OWNER IS NOT THE PARTY RESPONSIBLE FOR BMP MAINTENANCE AS PRESCRIBED)

INFILTRATION AREA CONSTRUCTION PROTECTION

FOR THE LONG—TERM FUNCTION OF THE INFILTRATING SYSTEMS. CARE MUST BE TAKEN IN THIS AREA DURING CONSTRUCTION. THE CONTRACTOR SHALL EMPLOY THE FOLLOWING MINIMUM BEST MANAGEMENT PRACTICES

- 1. THE INFILTRATION AREAS SHALL NOT BE USED AS A CONSTRUCTION SEDIMENTATION SYSTEM.
- 2. CONSTRUCTION EQUIPMENT, VEHICULAR TRAFFIC, PARKING OF VEHICLES, AND STOCKPILING OF CONSTRUCTION AND EARTH MATERIALS SHALL BE OUTSIDE THE LIMITS OF THE INFILTRATION AREA UNTIL INSTALLATION IS COMPLETED. THE SUBGRADE BENEATH THE SYSTEM SHALL NOT BE COMPACTED.
- 3. EXCAVATION FOR CONSTRUCTION OF THE INFILTRATION AREAS SHALL BE PERFORMED MANUALLY OR BY HYDRAULIC EXCAVATOR OR SOME OTHER SIMILAR MEANS TO ENSURE THAT THE EQUIPMENT IS NOT IN DIRECT CONTACT WITH THE NATURAL INFILTRATION EARTH MATERIAL AND DOES NOT CAUSE COMPACTION OF THE MATERIAL AND THE ENTIRE AREA IS TO BE SCARIFIED PRIOR TO INSTALLATION.
- 4. THE CONTRACTOR SHALL INSTALL TEMPORARY CONSTRUCTION FENCING AND EROSION CONTROLS AROUND THE PERIMETER OF THE INFILTRATION AREA TO PREVENT THE USE OF THIS AREA FOR ALL ACTIVITIES THAT MIGHT DAMAGE THE INFILTRATION CAPABILITIES OF THE AREA. THIS FENCING MAY BE REMOVED FOR BACKFILLING AND FINAL CONSTRUCTION.

SITE PLAN LEGEND (RIDOT):



- (CM) CUT AND MATCH PAVEMENT
- 4" LOAM AND SEEDING, TYPE 2
- (3.3.4)BRICK / SOLID BLOCK DOUBLE GRATE CATCH BASIN GRATE PARALLEL TO EDGE OF PAVEMENT
- HEAVY DUTY ROUND FRAME AND COVER
- HIGH CAPACITY FRAME AND GRATE (BICYCLE SAFE)
- PRECAST CONCRETE CURB
- (7.3.0)GRANITE CURB
- GRANITE RAMP STONE
- (7.6.0) CURB SETTING DETAIL

- (43.1.0) CEMENT CONCRETE SIDEWALK

(6W)

- 48.1.0 DETECTIBLE WARNING PANEL

(9.9.0) CONSTRUCTION ACCESS

- (20.1.0) PAVEMENT MARKINGS- ARROWS AND ONLY
- WHEELCHAIR RAMP
- WHEELCHAIR RAMP FOR LIMITED RIGHT-OF-WAY AREAS
- PLACEMENT

(43.2.0) BITUMINOUS CONCRETE SIDEWALK

6" PAVEMENT MARKINGS

- (4W) 4" PAVEMENT MARKINGS
 - 12" PAVEMENT MARKINGS-CROSSWALKS & STOP BAR

CENTERLINE (LAYOUT) STORM DRAIN ELECTRIC (UNDERGROUND)

DESCRIPTION

FIRE SERVICE

GAS

FOOTING DRAIN

OVERHEAD WIRE

PROPERTY LINE

TELEPHONE

WATER

CONTOUR

SPOT GRADE

SANITARY SEWER

SITE LIGHTING SERVICE

SPOT GRADE (BOT. OF CURB)

SPOT GRADE (TOP OF CURB)

SPOT GRADE (BOT. OF WALL)

SPOT GRADE (TOP OF WALL)

PRECAST CONC. CURB

CHAINLINK FENCE (CLF)

STOCKADE FENCE (STKF)

DOUBLE GRATE CATCH BASIN

CONCRETE THRUST BLOCK

FLARED END STRUCTURE

GATE VALVE AND CURB BOX

SOIL EVALUATION LOCATION

FIRE DEPARTMENT CONNECTION

POST INDICATOR VALVE (PIV)

ELECTRIC MANHOLE (EMH)

TRANSFORMER PAD

GENERATOR PAD

GROUND CLEANOUT

SIGHT LIGHT POLE

TRAFFIC FLOW DIRECTION

LIMIT OF DISTURBANCE

COMPOST SILT SOCKS

TO EXISTING

PAVEMENT SAWCUT & MATCH

RIDOT STD DETAIL REFERENCE

TELEPHONE MANHOLE (TMH)

TEST PIT LOCATION

HANDICAP SYMBOL (PRKG. SPACE)

BORING LOCATION

CATCH BASIN

DRAIN MANHOLE

SEWER MANHOLE

WATER SERVICE

UTILITY POLE

FIRE HYDRANT

WETLAND

SITE LEGEND

NEW

—— OHW ——

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<u>64</u>

 \times 64.00(BC)

× 64.50(TC)

64.50(BW)

64.50(TW)

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ABBREVIATIONS

CLDI

CLF

CTE

D.I.

ETR

EX.

F&I

INV.

MTE

NIC

NTS

PVC

RCP

VIF

HDPE

ESHWT

CAST IRON PIPE

CHAINLINK FENCE

DUCTILE IRON PIPE

EXISTING TO REMAIN

FURNISH AND INSTALL

INVERT ELEVATION

MATCH TO EXISTING

POLYVINYL CHLORIDE PIPE

REINFORCED CONCRETE PIPE

WATER QUALITY STRUCTURE

NOT IN CONTRACT

NOT TO SCALE

VERIFY IN FIELD

EXISTING

CEMENT LINED DUCTILE IRON PIPE

POINT OF CONNECTION TO EXISTING

HIGH DENSITY POLYETHYLENE PIPE

ESTIMATED SEASONAL HIGH WATER TABLE

(x.x.x.)

 $\times 64.00$

EXISTING

----- OHW -----

_____ S ____

----- SL -----

_____ T ____

____ W ____

_ _ - - 64 - - - - - -

 $\times 64.0$

 \times 64.0(BC)

 \times 64.5(TC)

64.5(BW)

64.5(TW)

—⊗——⊗——⊗——⊗-

☐ CB

CTB ⊳

☐ CB(DG)

SEV

GEN

 \ominus GCO

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REVISION

BY DATE PER RIDEM COMMENTS JPL 09/21/20

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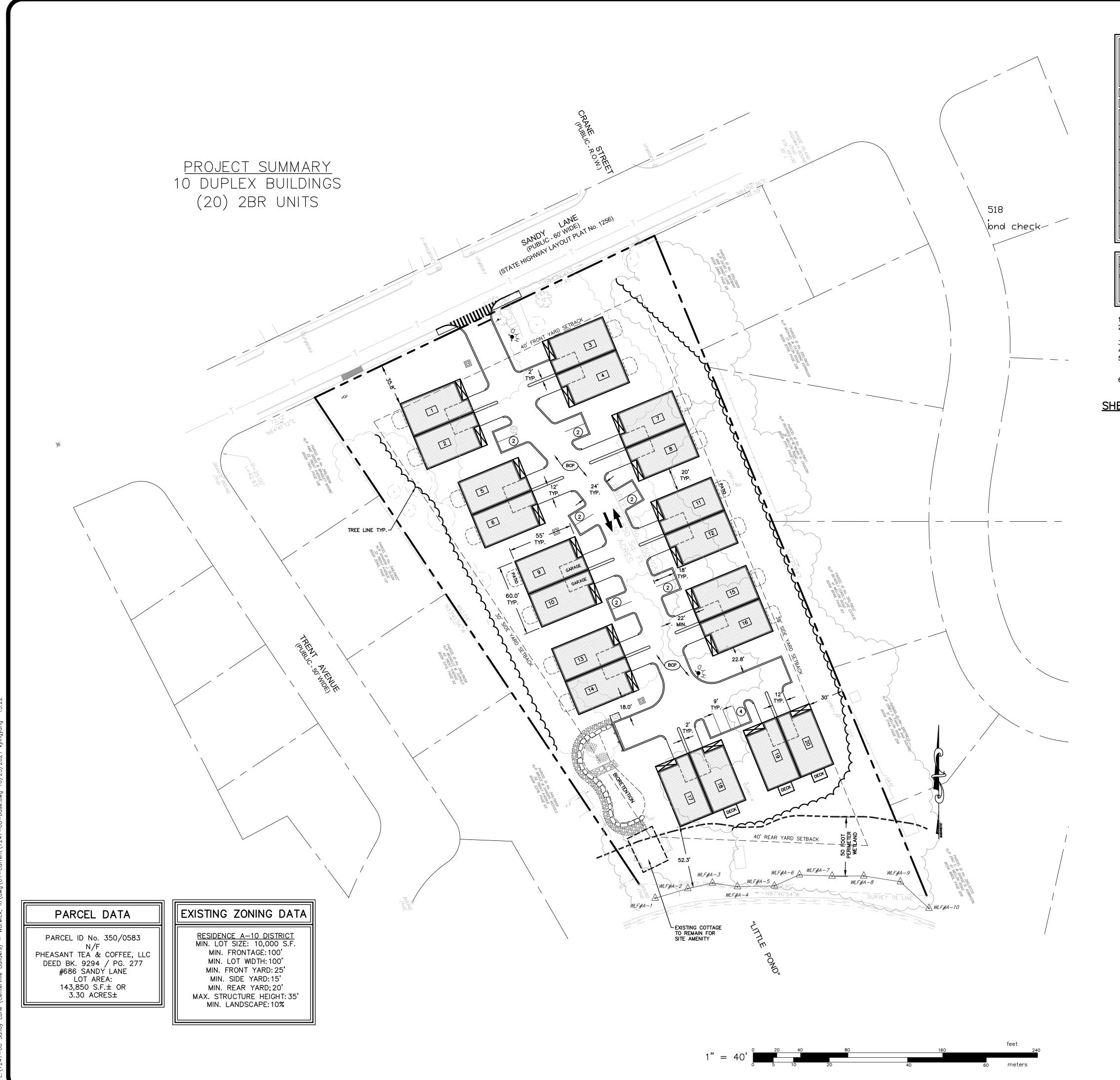
JOB NO. DRAWN BY K.Y.Y. 7247.00 DWG. NO. CHECKED S.S.H. 7247-00-Cover.dwg **APPROVED** S.S.H. SCALE:

SHEET

AS SHOWN

2 OF 13 SHEETS

AUGUST, 2021



DEVELOPMENT SUMMARY

A.P. 350, LOT 583 REZONE: RESIDENCE A-10- DISTRICT- PDR OVERLAY (1)

DESCRIPTION	REQUIRED	PROPOSED
MIN. LOT AREA	55,000 S.F.	±3.3 AC. (±140,000 S.F.)
MAX. DENSITY (DU/AC)	9 DU/AC	6.1 DU/AC (2)
MIN. LOT FRONTAGE	200'	>200'
MIN. LOT WIDTH	200'	>200'
MIN. FRONT YARD BUILDING SETBACK	40'	35.8'
MIN. SIDE YARD BUILDING SETBACK	30'	30.0'
MIN. REAR YARD BUILDING SETBACK	40'	48.5'±
MAX. BLDG HEIGHT	35'	< 35'
MIN. LANDSCAPE AREA	20%	63.7% (3)
MIN. DISTANCE BETWEEN BLDGS	30' (4)	20'

PARKING REQUIREMENTS		
DESCRIPTION	REQUIRED	PROPOSED
RESIDENTIAL - FIVE OR MORE UNITS	50 SPACES (5)	56 SPACES (6)

- 1) REFER ZONING, SECTION 308: OVERLAY DISTRICT REGULATIONS
 2) DENSITY CALCULATION:
 20 DU/3.3 ACRE = 6.1 DU/ACRES
 3) LANDSCAPE AREA = 91,723± S.F./143,850± S.F.= 63.7%
 4) REQUIRED BUILDING SEPARATION PER SECTION 308.2.D.
 5) PARKING REQUIRED PER SECTION 308.2.E:
 REQUIRED: 2.5 SPACES PER UNITS
 PROPOSED: 20 UNITS @ 2.5 SPACES PER UNIT = 50 SPACES
 6) PARKING IS PROPOSED WITHIN 15' OF RESIDENTIAL BUILDINGS AND REQUIRES RELIEF FROM SECTION 308.2.E

SHEET NOTES:

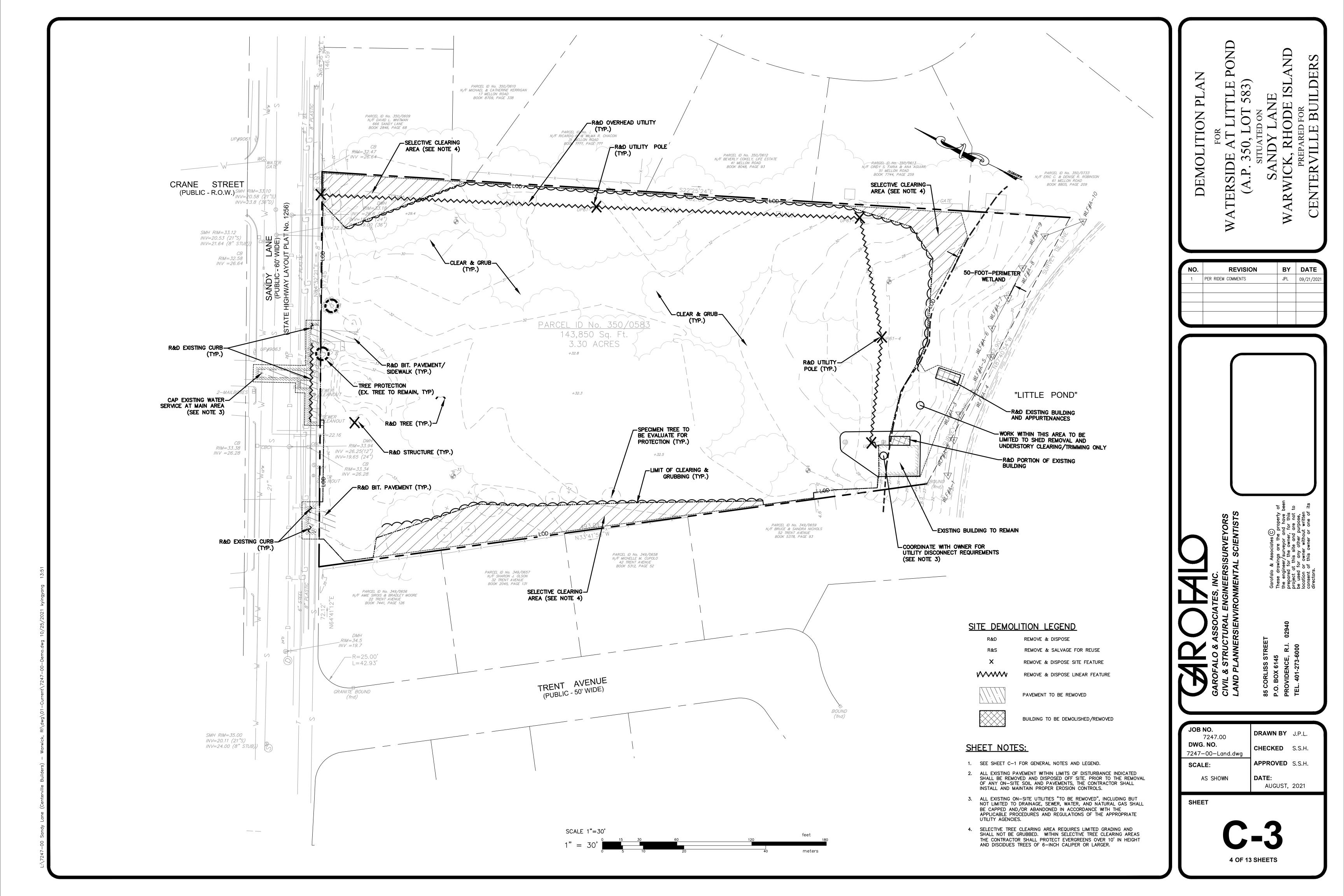
- 1. SEE SHEET C-1 FOR GENERAL NOTES AND LEGEND.
- THIS PLAN HAS BEEN DEVELOPED TO OUTLINE THE OVERALL PROJECT INTENT AND ZONING CONDITIONS ASSOCIATED WITH THE PROJECT. SEE SHEET C-4 FOR ADDITIONAL MATERIAL AND DIMENSIONAL DATA.
- 3. REFER TO DEVELOPMENT SUMMARY FOR REQUIRED DIMENSIONAL RELIEF FOR THE PROJECT.
- 4. THE EXISTING COTTAGE ON THE PROPERTY IS INTENDED TO BE REPAIRED/RESTORED AS A SITE AMENITY/FEATURE. THE STRUCTURE SHALL NOT BE UTILIZED AS A DWELLING AND THE DIMENSIONAL REQUIREMENTS OUTLINED HEREIN DO NOT REFLECT THE CONDITIONS OF THE COTTAGE STRUCTURE.

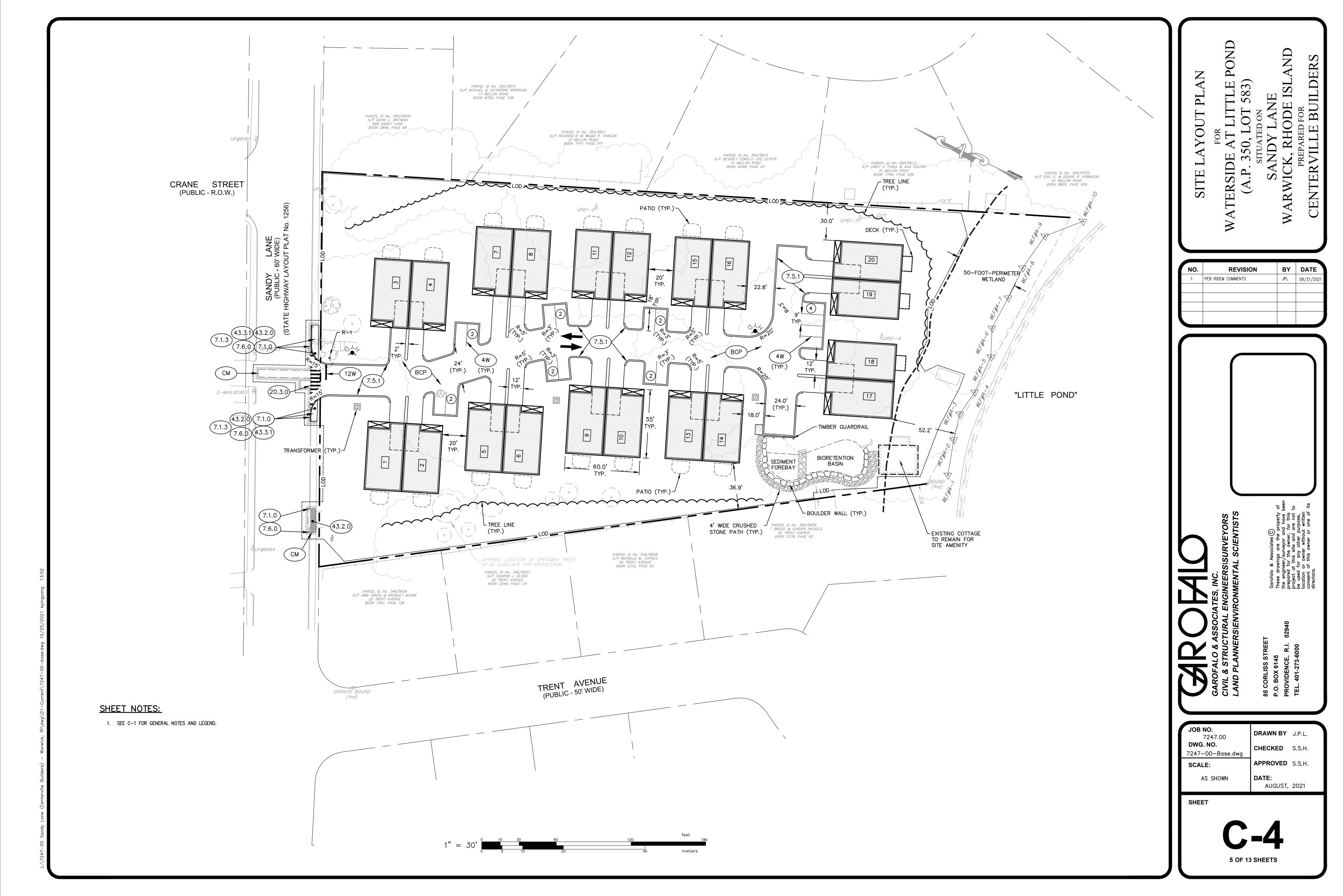
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	PER RIDEM COMMENTS	JPL	09/21/2021

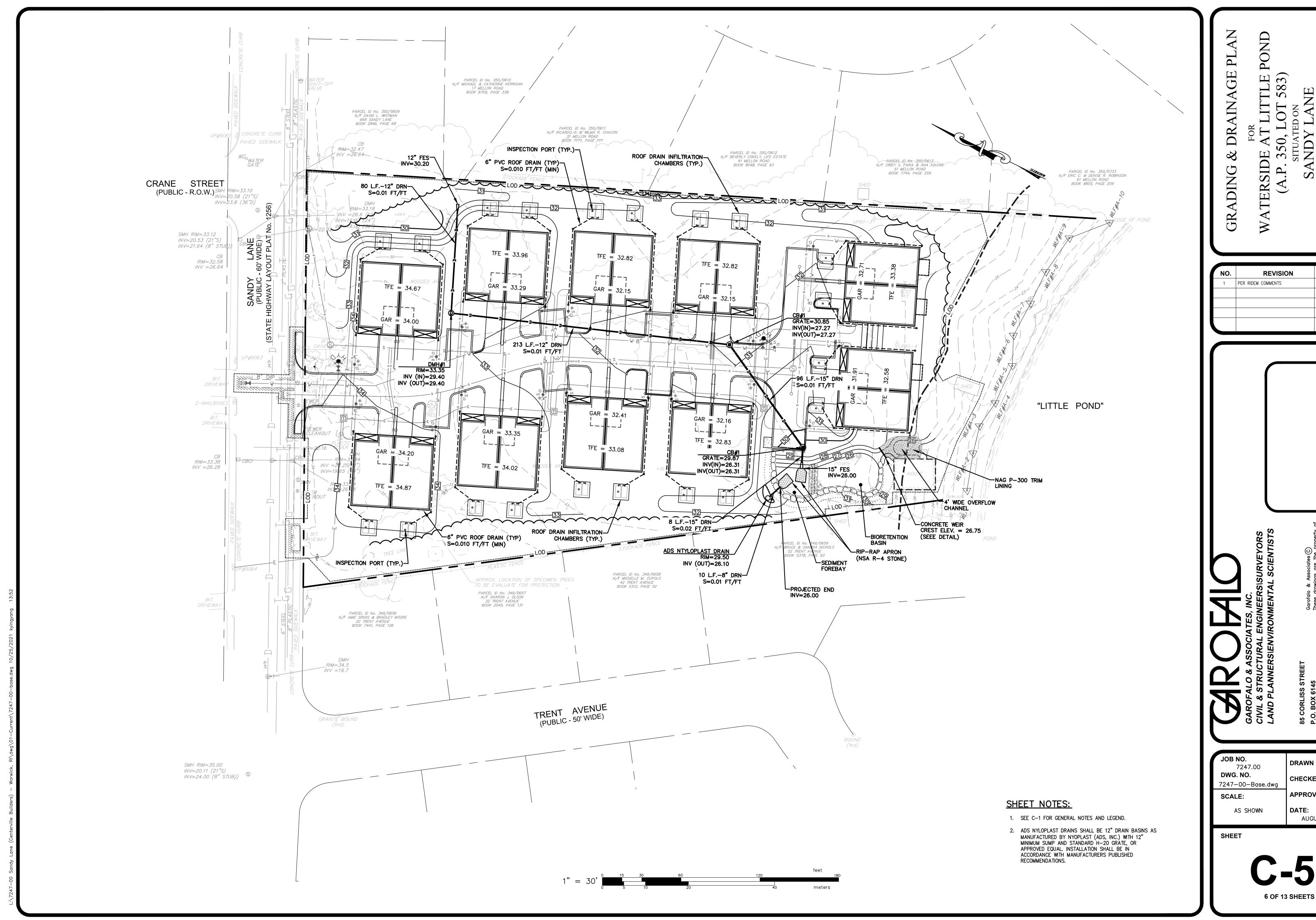
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DWG. NO.	CHECKED	S.S.H.
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SCALE:	APPROVED	S.S.H.

AUGUST, 2021

SHEET





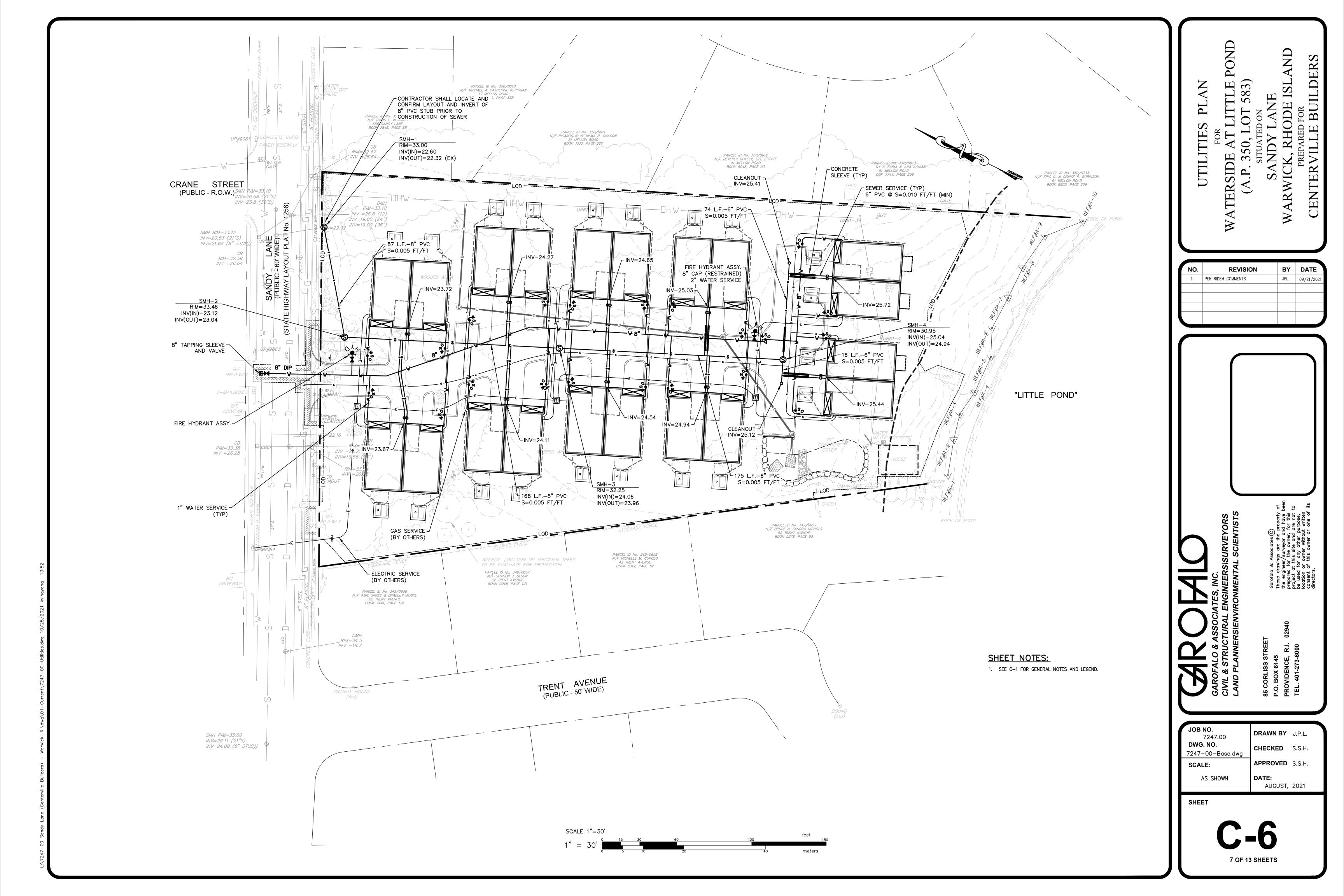


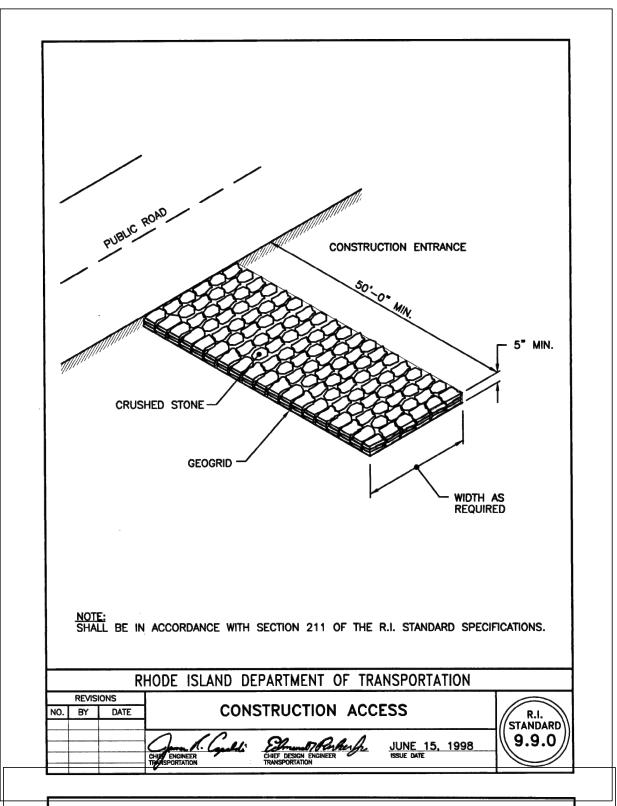
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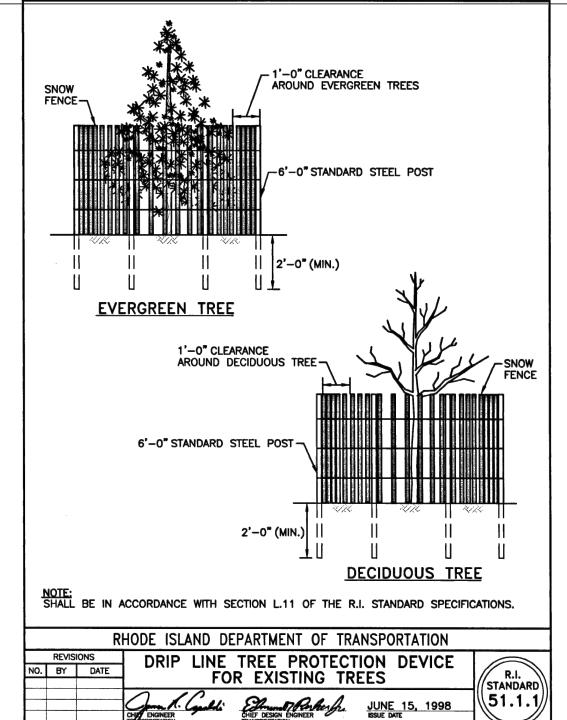
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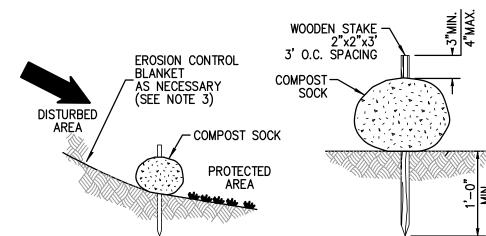
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AUGUST, 2021









COMPOST SILT SOCK DETAIL NOT TO SCALE

1. COMPOST SOCK SHALL BE INSTALLED PER MANUFACTURERS RECOMMENDATIONS. COMPOST SOCK SHALL BE A MINIMUM OF 8" IN DIAMETER.

- 2. COMPOST MATERIAL TO BE DISPERSED ON SITE, AS DETERMINED BY THE ENGINEER. 3. WHEN PLACING COMPOST SOCK ON SLOPES, USE EROSION CONTROL BLANKET IF SPECIFIED
- 4. ALWAYS INSTALL COMPOST SOCK PERPENDICULAR TO SLOPE AND ALONG CONTOUR LINES.
- 6. WHERE COMPOST SOCK IS INSTALLED ON PAVEMENT SHALL BE ADEQUATELY HIGH AT 8'-0"

TEMPORARY SEDIMENT TRAP AND OUTLET DETAIL HAY BALE/SILT SOCK EROSION CONTROL STOCKPILÉ MATERIAL EXPANSION-RESTRAINT INSTALL HI-FLOW SILTSACK PRIOR TO CONSTRUCTION REMOVE UPON COMPLETION CATCH BASIN OF UTILITY INSTALLATION 5' MAXIMUM FROM TOE OF SLOPE —— SILTSACK®AT CATCH BASIN DETAIL 5. REMOVE SEDIMENT FROM THE UP SLOPE SIDE OF THE COMPOST SOCK WHEN ACCUMULATION HAS REACHED 1/2 OF THE EFFECTIVE HEIGHT OF THE COMPOST SOCK. STOCKPILE DETAIL NOT TO SCALE NOT TO SCALE



EROSION CONTROL & SOIL STABILIZATION PROGRAM:

1. DENUDED SLOPES SHALL NOT BE LEFT EXPOSED FOR EXCESSIVE PERIODS OF TIME.

- FUTURE INFILTRATION CHAMBER (AVOID COMPACTION IN THIS AREA)

TOPSOIL

SELECTIVE CLEARING AREA -

- PROTECT BMP WITH COMPOST SILT

SOCK UNTIL SITE FULLY STABILIZED

(AVOID COMPACTION IN THIS AREA)

- LIMIT OF DISTURBANCE (TYP.)

(SEE SHEET C-3 REQUIREMENTS)

PARCEL ID No. 350/0612 N/F BEVERLY COKELY, LIFE ESTATE 41 MELLON ROAD BOOK 8048, PAGE 93

SEDIMENT MEASUR WATERSHED 3

(SEE SHEET C-3 REQUIREMENTS)

<u>WATERSHED LIMIT O</u> SEDIMENT MEASURE (TYP

- ALL DISTURBED SLOPES EITHER NEWLY CREATED OR EXPOSED PRIOR TO OCTOBER 15, SHALL BE SEEDED OR PROTECTED BY THAT DATE FOR ANY WORK COMPLETED DURING EACH CONSTRUCTION YEAR.
- 3. TEMPORARY TREATMENTS SHALL CONSIST OF A HAY, STRAW, OR FIBER MULCH OR PROTECTIVE COVERS SUCH AS A MAT OR FIBER LINING (BURLAP, JUTE, FIBERGLASS NETTING, EXCELSIOR BLANKETS). THEY SHALL BE INCORPORATED INTO THE WORK AS WARRANTED OR AS ORDERED BY THE ENGINEER.
- 4. HAY OR STRAW APPLICATIONS SHOULD BE IN THE AMOUNT OF 2000 LBS/ACRE.
- 5. ALL HAYBALES OR TEMPORARY PROTECTION SHALL REMAIN IN PLACE UNTIL AN ACCEPTABLE STAND OF GRASS OR APPROVED GROUND COVER IS ESTABLISHED.
- 6. THE TOPSOIL SHALL HAVE A SANDY LOAM TEXTURE RELATIVELY FREE OF SUBSOIL MATERIAL, STONES, ROOTS, LUMPS OF SOIL, TREE LIMBS, TRASH OR CONSTRUCTION DEBRIS.
- 7. THE SEED MIX SHALL BE INOCULATED WITHIN TWENTY FOUR (24) HOURS, BEFORE MIXING AND PLANTING, WITH APPROPRIATE INOCULUM FOR EACH VARIETY.
- 8. THE DESIGN MIX FOR TEMPORARY EROSION CONTROL AND SOIL STABILIZATION SHALL BE COMPRISED OF THE FOLLOWING:

% BY WEIGHT CREEPING RED FESCUE ASTORIA BENTGRASS BIRDSFOOT TREEFOIL PERENNIAL RYEGRASS APPLICATION RATE 100 LBS/ACRE

LIMING AND FERTILIZING AS REQUIRED TO COMPLIMENT OR UPGRADE EXISTING CONDITIONS.

- 9. THE CONTRACTOR MUST REPAIR AND/OR RESEED ANY AREAS THAT DO NOT DEVELOP WITHIN THE PERIOD OF ONE (1) YEAR, AND HE SHALL DO SO AT NO ADDITIONAL EXPENSE.
- 10. THE NORMAL ACCEPTABLE SEASONAL SEEDING DATES ARE APRIL 1ST THROUGH OCTOBER
- 11. STABILIZATION OF ONE FORM OR ANOTHER AS DESCRIBED ABOVE SHALL BE ACHIEVED WITHIN FIFTEEN (15) DAYS OF FINAL GRADING.
- 12. STOCKPILES OF TOPSOIL SHALL NOT BE LOCATED NEAR WATERWAYS OR FLOOD PLAINS. THEY SHALL HAVE SIDE SLOPES NO GREATER THAN THIRTY PERCENT (30%) AND STOCKPILES SHALL ALSO BE SEEDED AND/OR STABILIZED AND COMPLETELY ENCIRCLED WITH A CONTINUOUS LINE OF STAKED HAYBALES AND/OR SILTFENCE. (SEE DETAIL)
- 13. ON BOTH STEEP AND LONG SLOPES, CONSIDERATION SHOULD BE GIVEN TO "CRIMPING" OR "TRACKING" TO TACK DOWN MULCH APPLICATIONS.

SEQUENCE AND STAGING OF LAND **DISTURBING ACTIVITIES:**

1. SURVEY AND STAKE THE DRAINAGE STRUCTURES, LIMIT OF WORK AND SEDIMENTATION BARRIERS.

COMPOST SILT SOCK (TYP.)

SHEET NOTES:

1. SEE C-1 FOR GENERAL NOTES AND LEGEND.

RESET COMPOST SILT SOCK OUTSIDE OF PERIMETER WETLAND AFTER COMPLETION

OF RESTORATION WORK

"LITTLE POND"

 $\frac{1}{2}$ COMPOST SILT SOCK (TYP.)

- <u>RESTORATION AREA</u> WORK WITHIN THIS AREA TO BE

LIMITED TO SHED REMOVAL AND UNDERSTORY CLEARING/TRIMMING ONLY

TO REMAIN

- PLACE SEDIMENTATION BARRIERS (SILT SOCK) AS SHOWN ON THE PLANS AND STAKED OUT IN THE FIELD. IN NO CASE IS THE LIMIT OF WORK TO EXTEND BEYOND THE
- BEGIN GRADING AND DRAINAGE WORK (CLEARING AND GRUBBING, EXCAVATING AND GRADING, ETC.). TOP SOIL TO BE STRIPPED AND STOCKPILED IN APPROVED AREAS. THE STOCKPILES ARE TO BE PROTECTED BY A ROW OF SEDIMENTATION BARRIER AND COVERED OR TEMPORARILY SEEDED.
- 4. INSTALL DRAINAGE SYSTEM. INSTALL SEDIMENT PROTECTION IMMEDIATELY AFTER INSTALLATION.
- 5. COMPLETE SITE GRADING.

SEDIMENTATION BARRIERS.

- INSTALL BMPS IN ACCORDANCE WITH PLANS. SEDIMENT CONTROLS SHALL BE MAINTAINED AT THE BMP STRUCTURES UNTIL THE BINDER COURSE OF PAVEMENT IS INSTALLED AND DISTURBED AREAS ARE STABILIZED WITH VEGETATION.
- COMPLETE LANDSCAPING AND PERMANENT STABILIZATION. SWEEP THE ROADWAY AREAS TO REMOVE ALL SEDIMENTS.
- 8. FLUSH AND/OR REPAIR DRAINAGE AREAS AS NEEDED.
- REMOVE ALL TEMPORARY SOIL EROSION AND SEDIMENTATION CONTROL MEASURES FOLLOWING VEGETATIVE ESTABLISHMENT OF ALL DISTURBED AREAS.

SEDIMENTATION CONTROL PROGRAM:

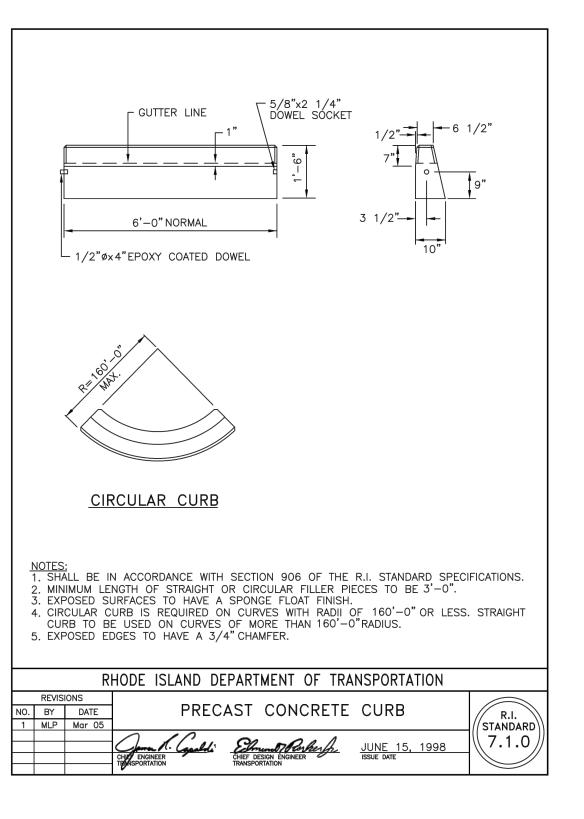
- 1. EXTREME CARE SHALL BE EXERCISED SO AS TO PREVENT ANY SEDIMENTS FROM ENTERING THE STATE HIGHWAY OR ADJOINING PROPERTIES.
- 2. BANKS OR SLOPES OVER 5% SHALL BE SEEDED AS SOON AS POSSIBLE AND SHALL BE
- PROTECTED WITH A HAY, STRAW OR FIBER MULCH. 3. DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING
- DRAINAGE AND RUNOFF FLOW DURING STORMS AND PERIODS OF RAINFALL.
- 4. SEDIMENTATION CONTROL DEVICES SHALL BE INSPECTED CLOSELY AND MAINTAINED PROMPTLY AFTER EACH RAINFALL.
- 5. CARE SHALL BE TAKEN SO AS NOT TO PLACE "REMOVED SEDIMENTS" WITHIN THE PATH OF EXISTING, NEWLY CREATED (BOTH TEMPORARY AND PERMANENT) OR PROPOSED
- ADDITIONAL SILT SOCKS SHALL BE LOCATED AS CONDITIONS WARRANT OR AS DIRECTED BY THE ENGINEER.

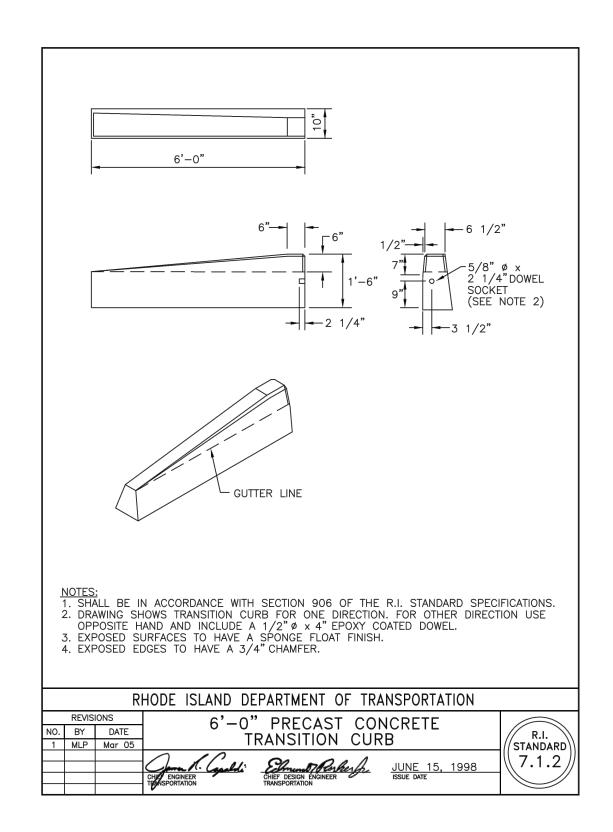
WATERCOURSES OR THOSE AREAS SUBJECTED TO STORM WATER FLOW.

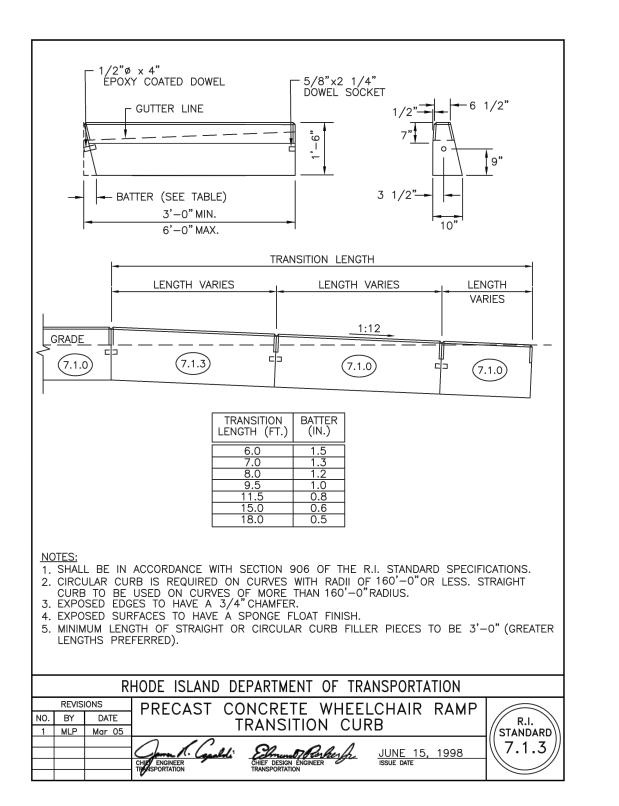
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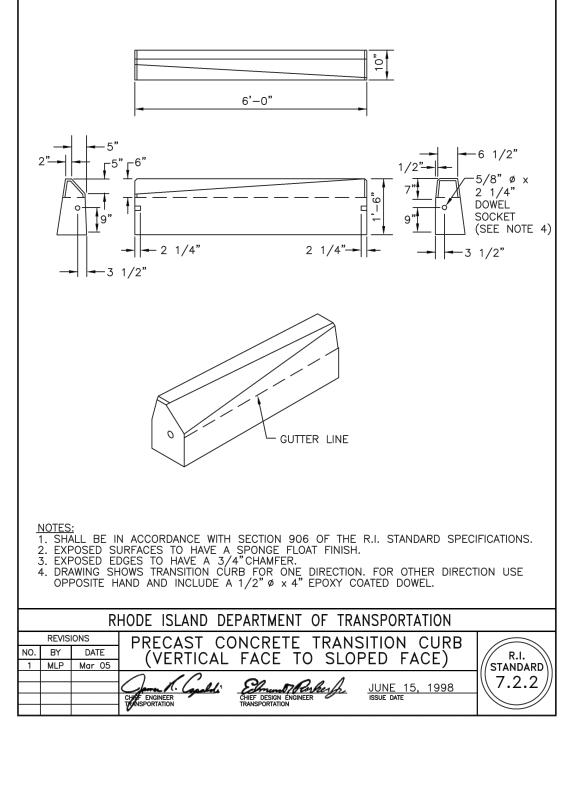
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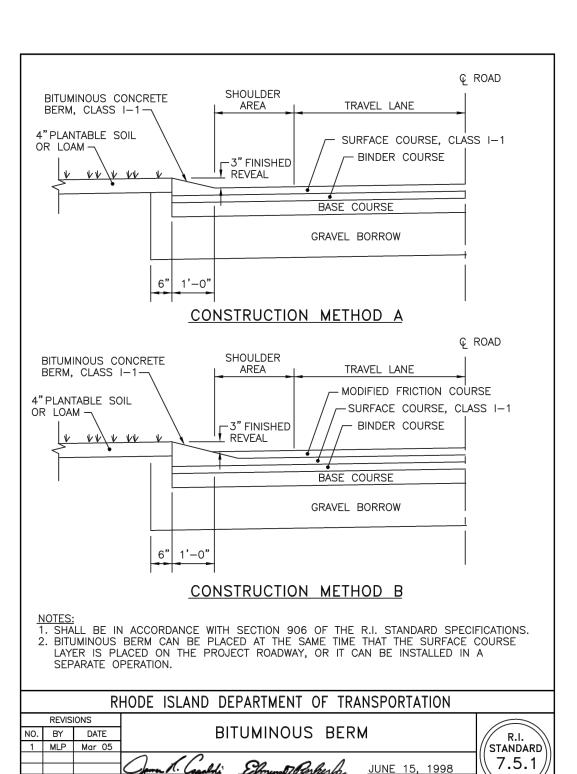
APPROVED S.S.H. AS SHOWN AUGUST, 2021

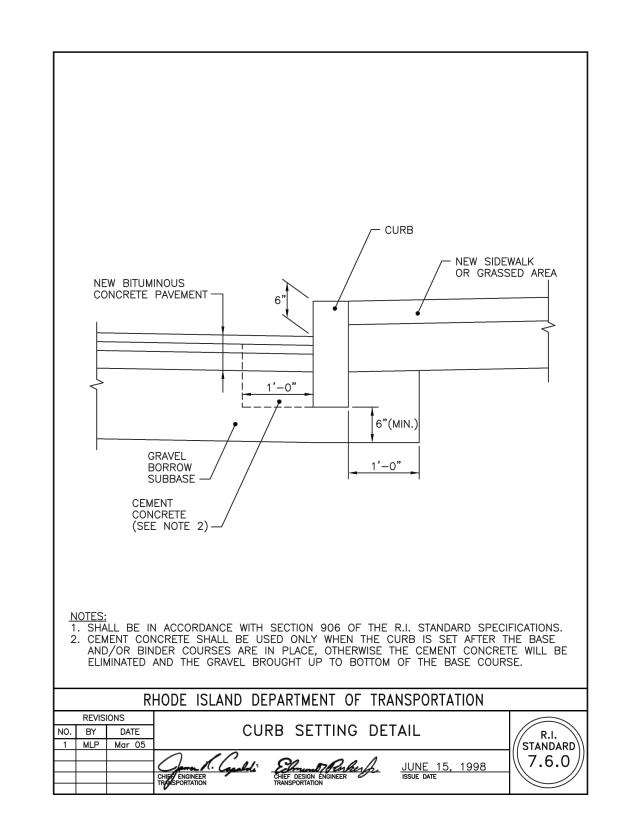


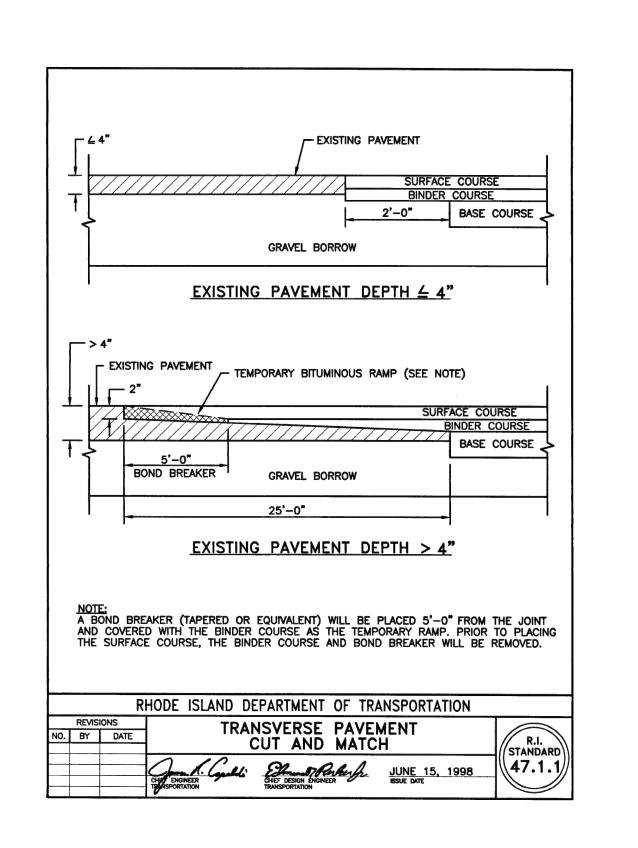


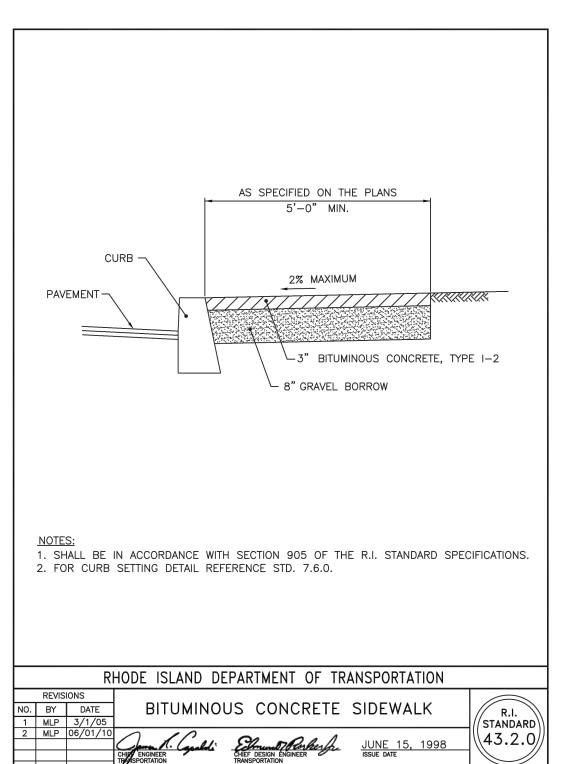


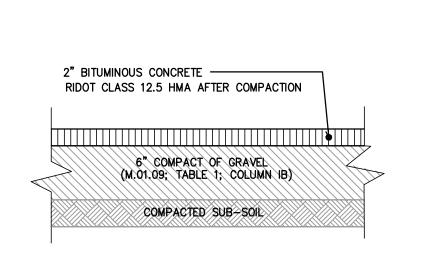




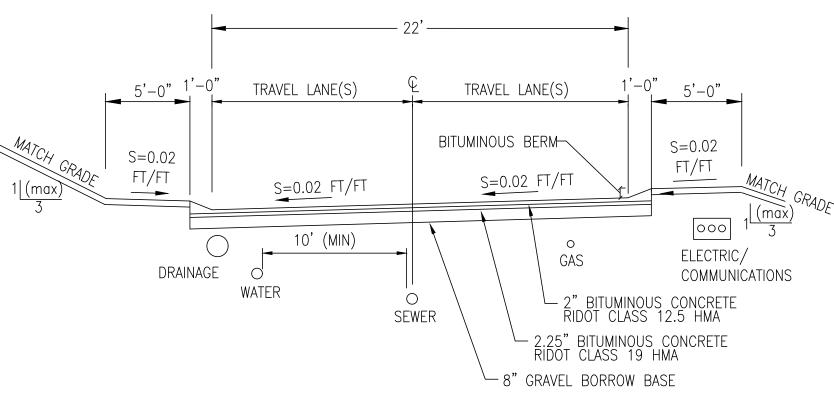












ROAD TYPICAL SECTION

NOT TO SCALE



- 2. METHODS AND MATERIALS USED FOR ROAD CONSTRUCTION SHALL CONFORM TO THE CURRENT CONSTRUCTION STANDARDS AND SPECIFICATIONS OF THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION.
- . GRAVEL BORROW SHALL CONFORM TO STANDARD SPECIFICATION SUBSECTION M.01.0; TABLE 1, COLUMN 1B.
- 4. SUB-SOIL FILLS SHALL BE COMPRISED OF SELECT GRANULAR MATERIALS, CLASSIFIED AS SM OR MORE GRANULAR BY ASTM D 2487 AND PLACED IN 12-INCH MAXIMUM LIFTS AND SHALL BE COMPACTED TO 98% OF THE MAXIMUM STANDARD PROTECTOR DENSITY AS DETERMINED BY THE MOISTURE-DENSITY RELATIONSHIP TEST METHOD ASTM D689.



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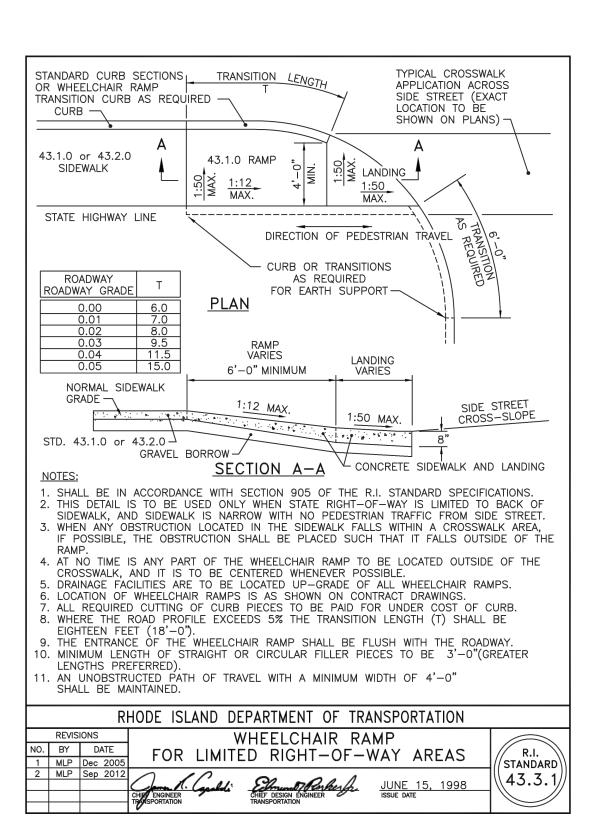
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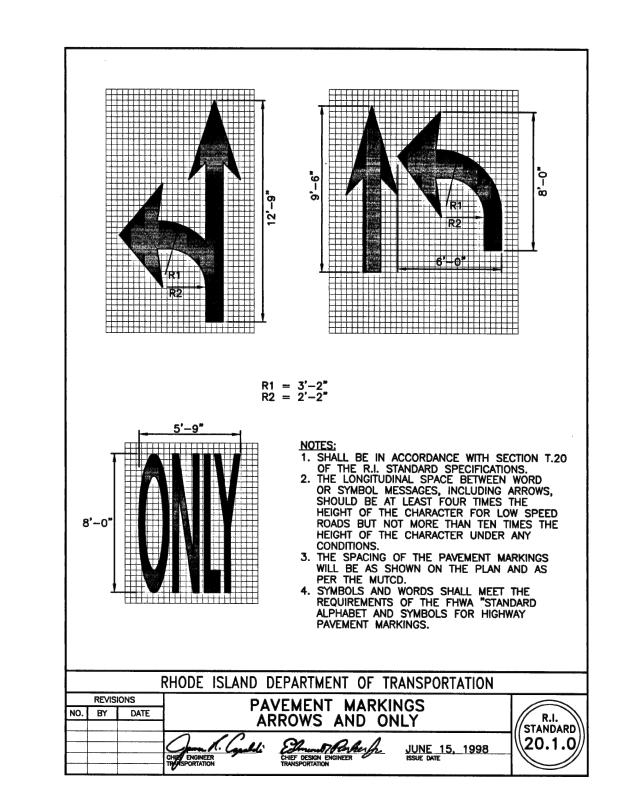
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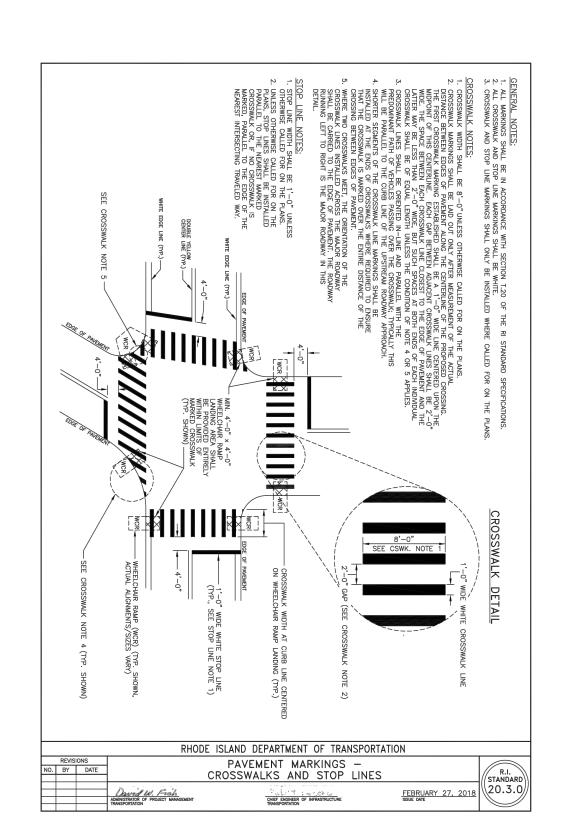
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AUGUST, 2021

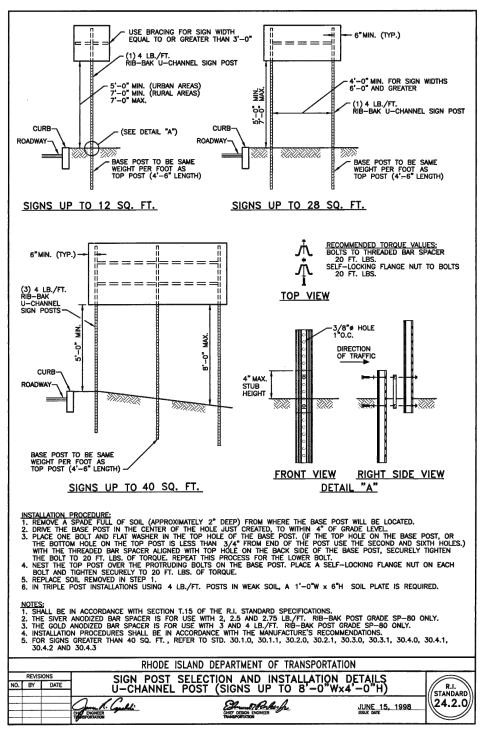
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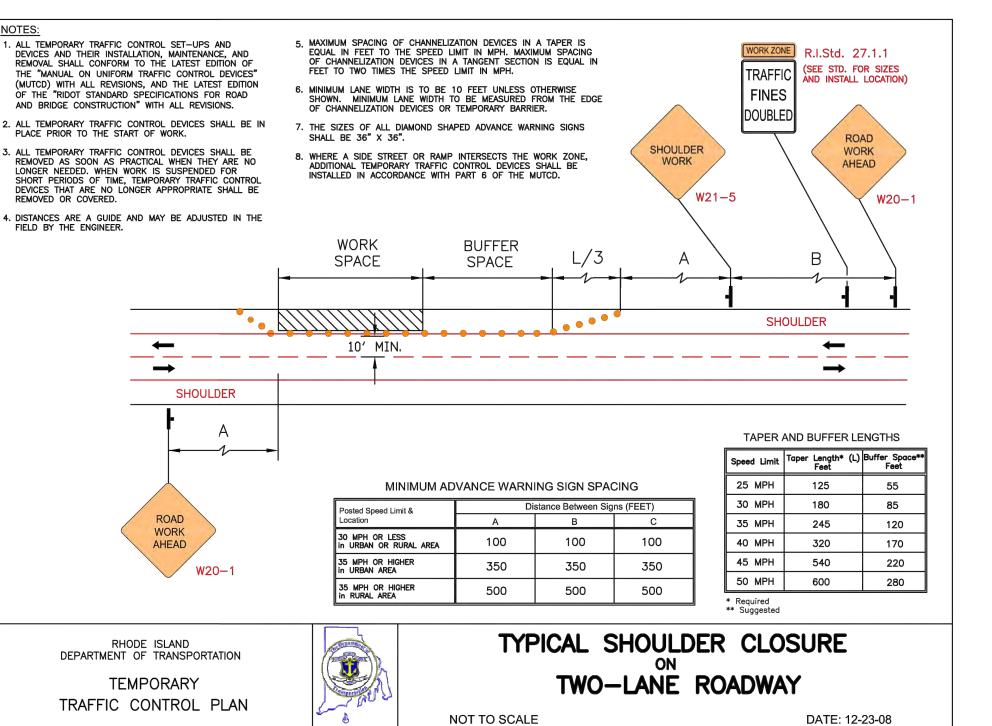
C-89 OF 13 SHEETS

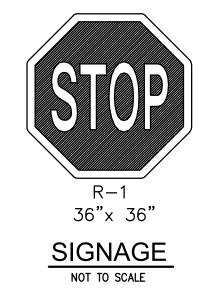












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10 OF 13 SHEETS

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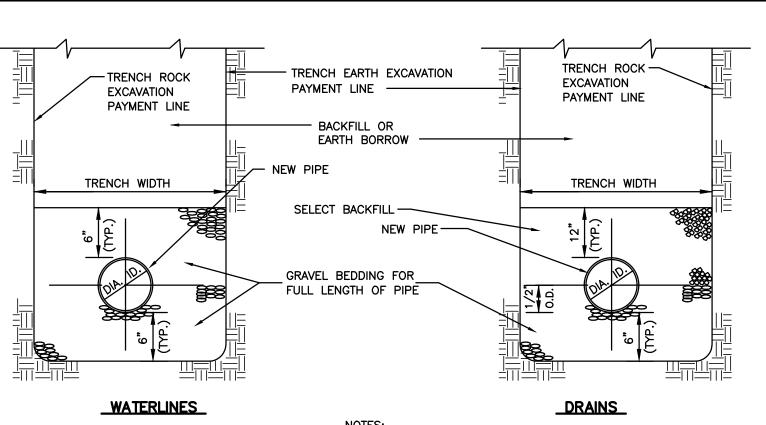
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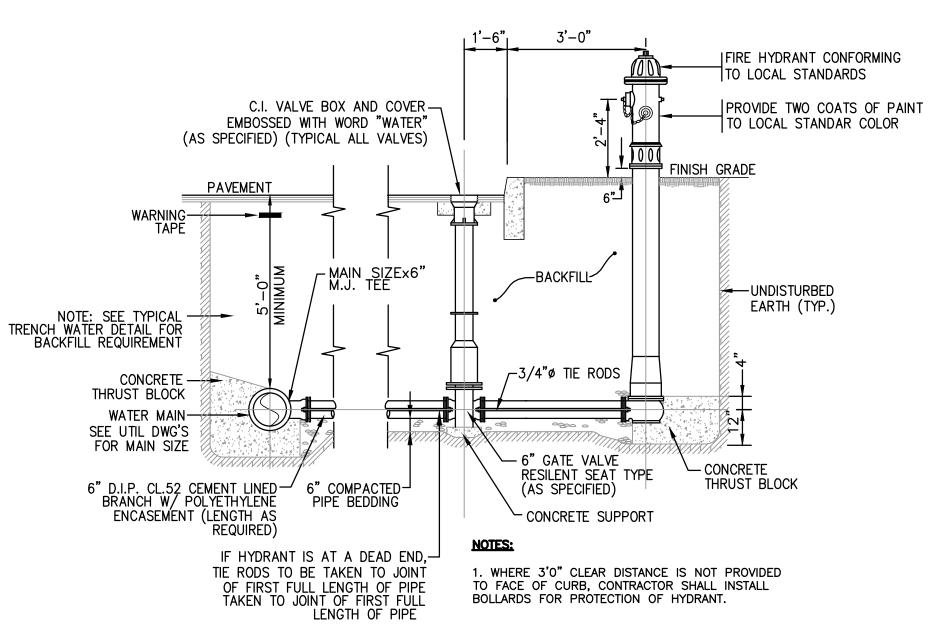
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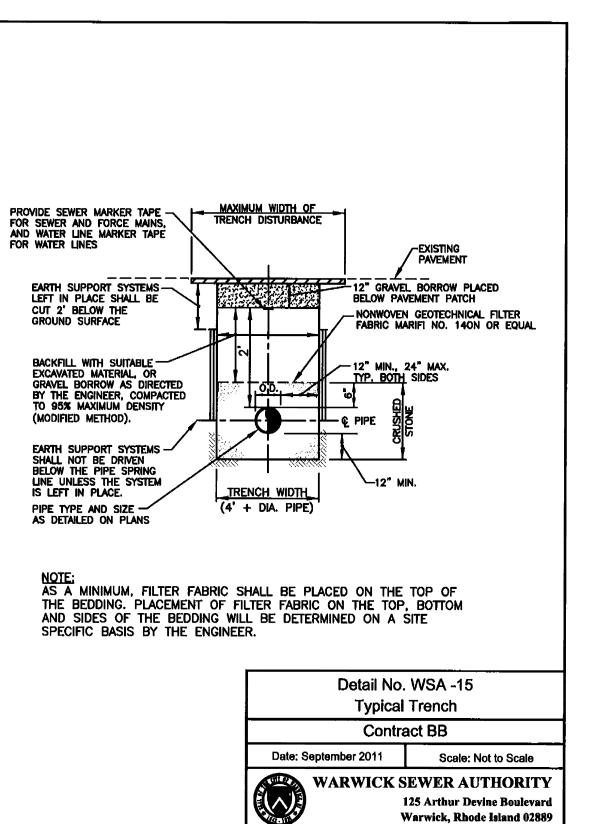
1. METALIZED 2" WIDE DETECTABLE IDENTIFICATION TAPE SHALL BE 2. SEE DETAIL BELOW FOR SEWER TRECH REQUIREMENTS.

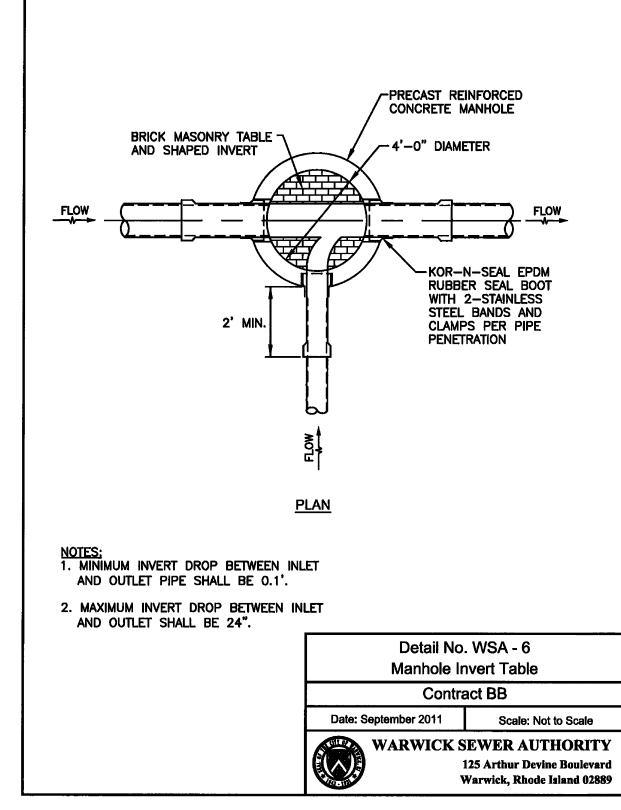
INSTALLED IN ACCORDANCE WITH UTILITY COMPANY STANDARDS AND ALL APPLICABLE CODES.

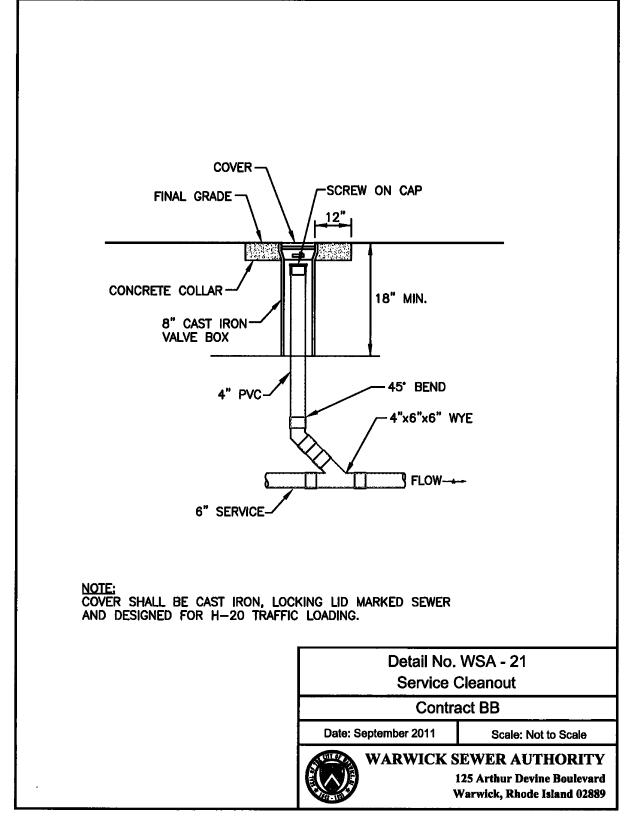
UTILITY TRENCH



HYDRANT ASSEMBLY DETAIL

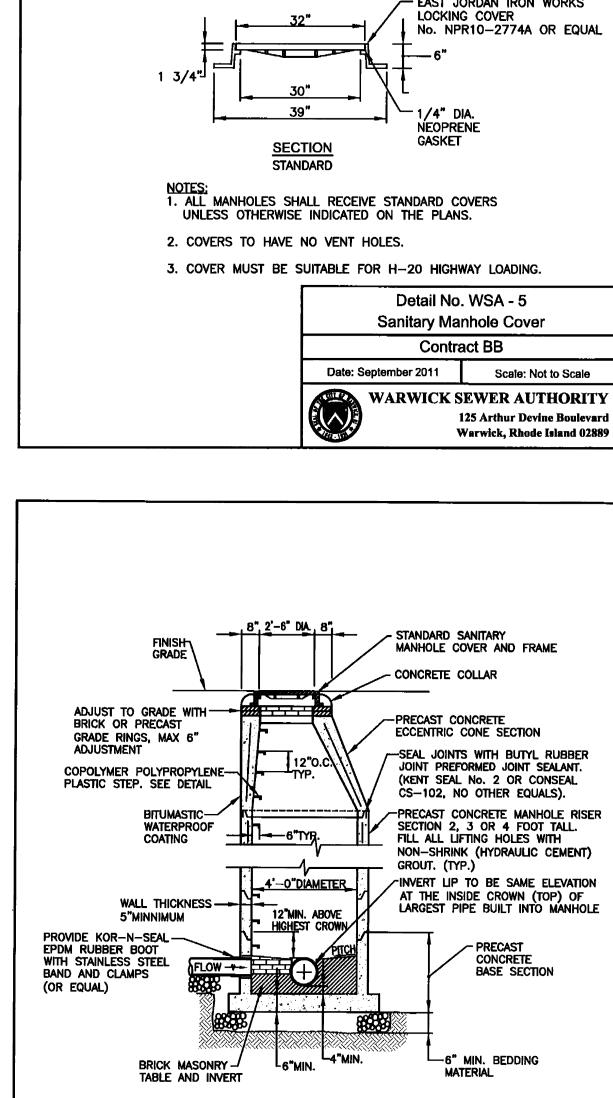


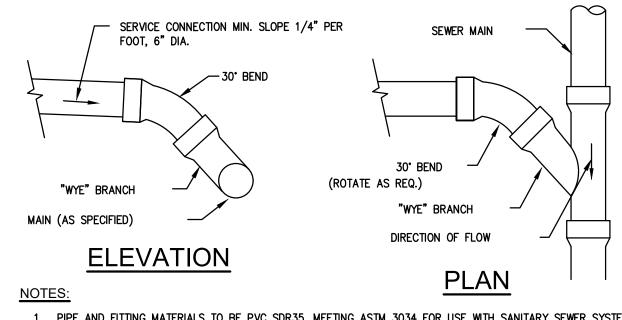




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WATER LINE

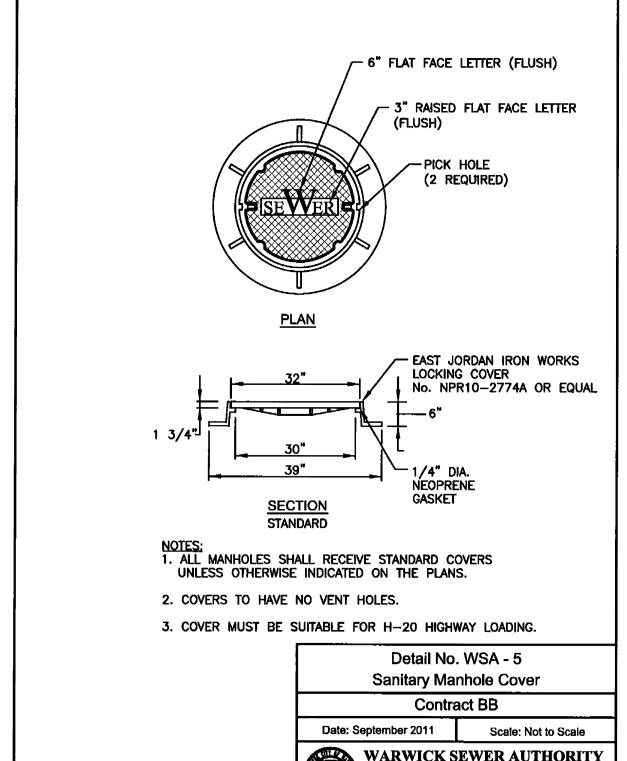


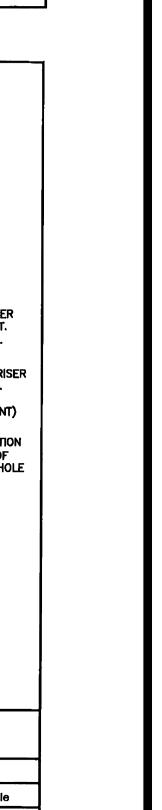


1. PIPE AND FITTING MATERIALS TO BE PVC SDR35, MEETING ASTM 3034 FOR USE WITH SANITARY SEWER SYSTEMS. 2. REFER TO "TRENCH SECTION (SEWER)" FOR TRENCHING REQUIREMENTS.

NOT TO SCALE

SEWER SERVICE CONNECTION DETAIL





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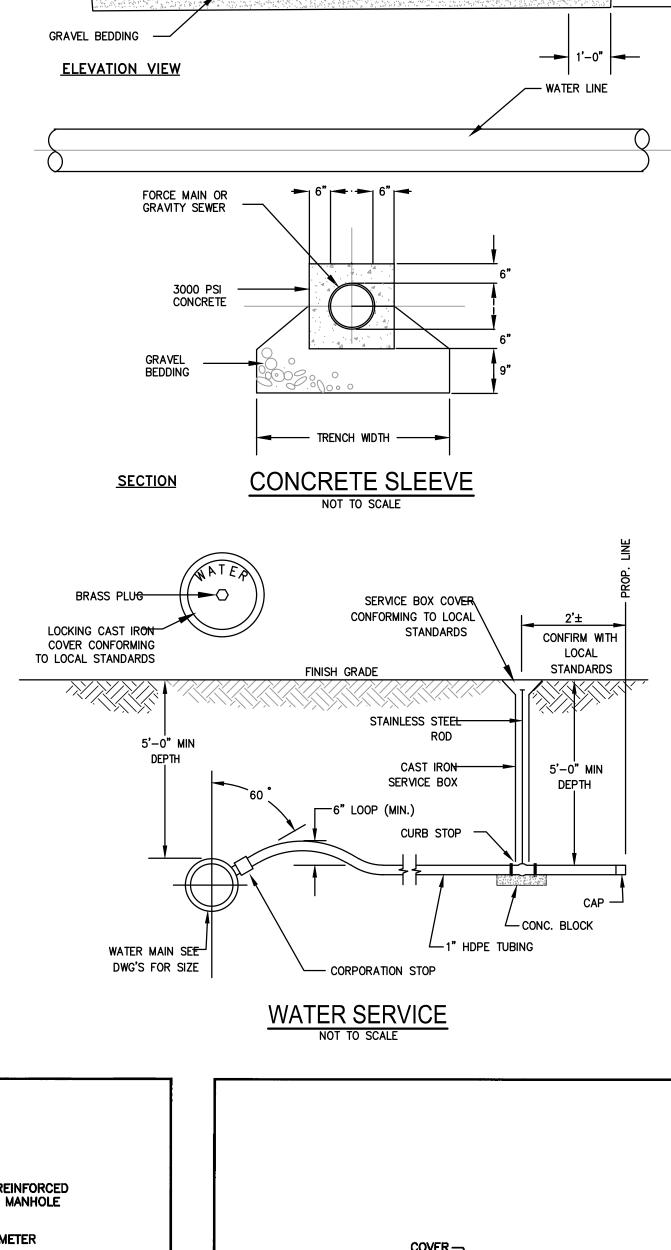
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REVISION

JOB NO. **DRAWN BY** J.P.L. 7247.00 DWG. NO. **CHECKED** S.S.H. 7247-00-Details.dwg **APPROVED** S.S.H. SCALE: AS SHOWN AUGUST, 2021

SHEET

11 OF 13 SHEETS



CONCRETE

FORCE MAIN OR GRAVITY SEWER

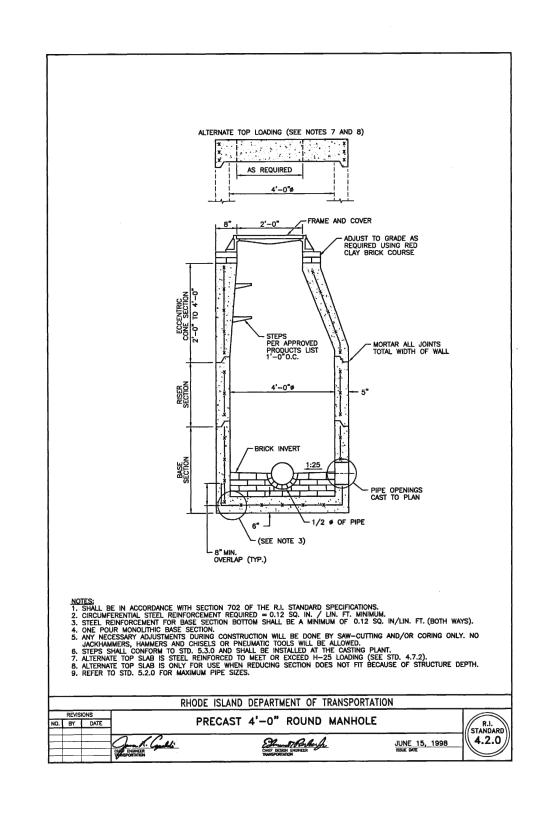
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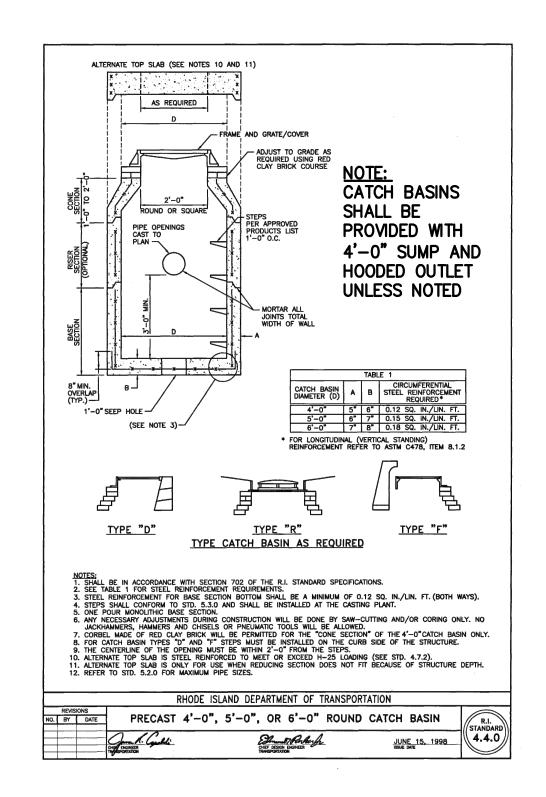
1. ALL JOINTS SHALL BE TONGUE AND GROOVE WITH BUTYL RUBBER JOINT PREFORMED SEALANT. . ALL LIFTING HOLES OR INDENTS ARE TO BE SEALED WITH NON-SHRINKING

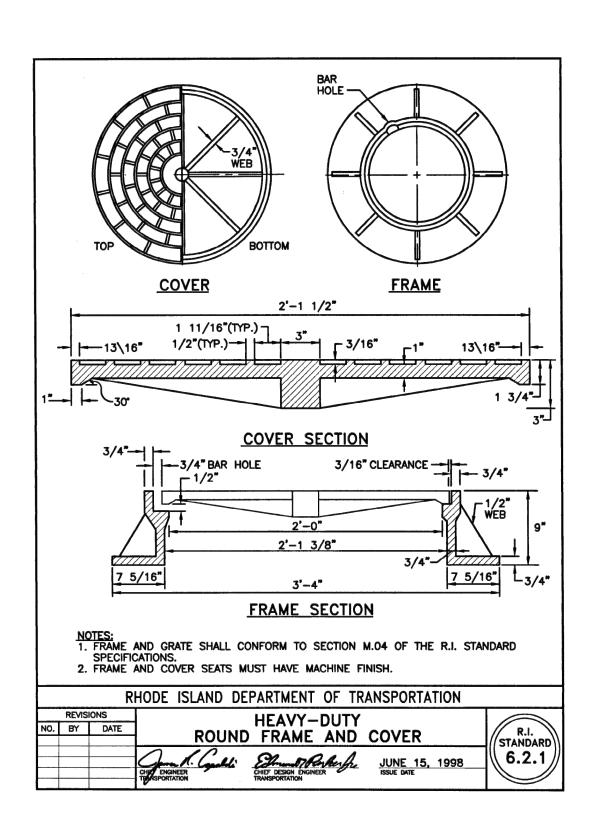
(HYDRAULIC CEMENT) GROUT.

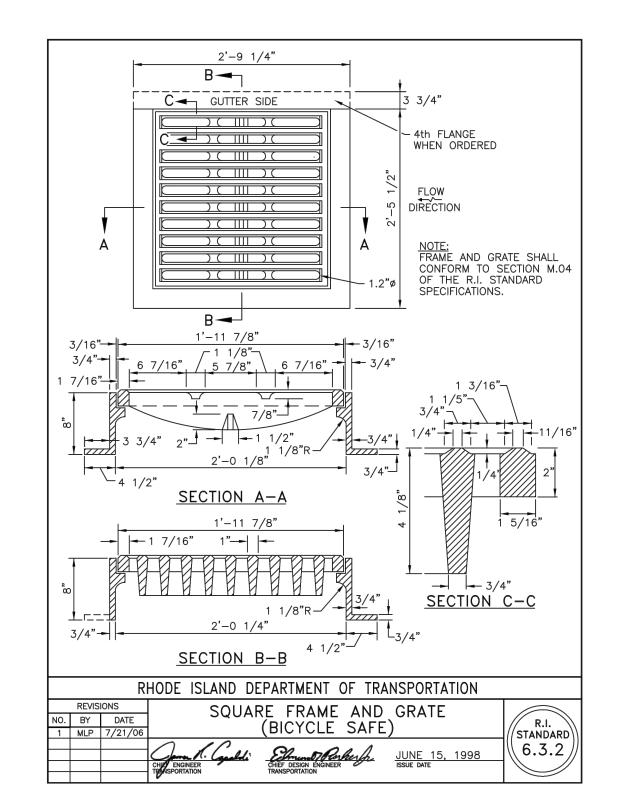
Detail No. WSA - 3 Sanitary Manhole Contract BB Date: September 2011 Scale: Not to Scale WARWICK SEWER AUTHORITY

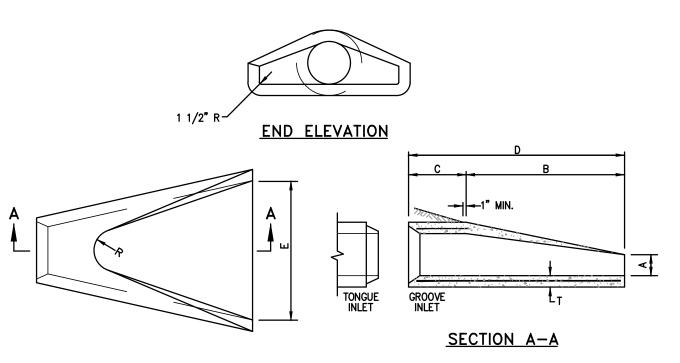
125 Arthur Devine Boulevard Warwick, Rhode Island 02889









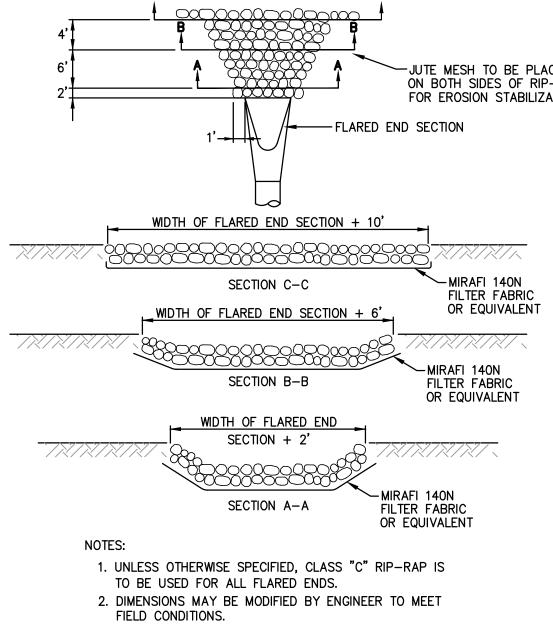


	DIMENSIONS						REINFORCEMENT	
DIA.	A	В	С	D	E	R	т	ONE LAYER REINFORCEMENT IN CENTER OF WALL
					_	.,	,	MIN. AREA OF EACH WAY (SQ. IN./FT.)
1'-0"	4"	2'-0"	4'-0 7/8"	6'-0 7/8"	2'-0"	9"	2"	0.048
1'-3"	6"	2'-3"	3'-10"	6'-1"	2'-6"	11"	2 1/4"	0.054
1'-6"	9"	2'-3"	3'-10"	6'-1"	3'-0"	12"	2 1/2"	0.060
2'-0"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	1'-2"	3"	0.072
2'-6"	1'-0"	4'-6"	1'-7 3/4"	6'-1 3/4"	5'-0"	1'-3"	3 1/2"	0.084
3'-0"	1'-3"	5'-3"	2'-10 3/4"	8'-1 3/4"	6'-0"	1'-8"	4"	0.096
3'-6"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	1'-10"	4 1/2"	0.108
4'-0"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	1'-10"	5"	0.120
4'-6"	2'-3"	5'-5"	2'-11"	8'-4"	7'-6"	2'-0"	5 1/2"	0.132
5'-0"	2'-6"	5'-0"	3'-3"	8'-3"	8'-0"	2'-0"	6"	0.144

NOTE: SHALL BE IN ACCORDANCE WITH SECTION 701 OF THE R.I. STANDARD SPECIFICATIONS.

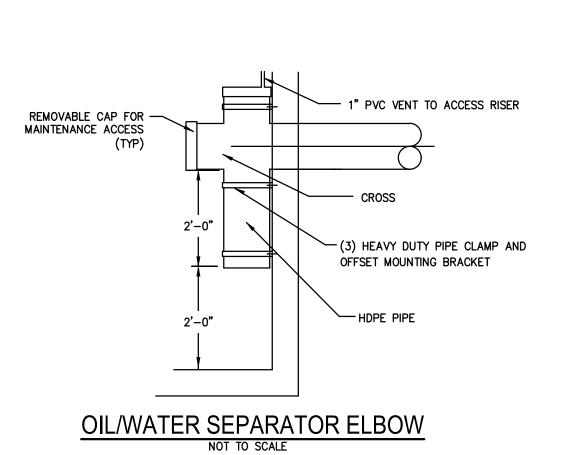
PRECAST CONCRETE FLARED END SECTION

NOT TO SCALE (RIDOT STD. 2.3.0)



- 3. UNLESS OTHERWISE SPECIFIED, DUMPED RIP-RAP SHALL
- BE USED.

RIP RAP AT FLARED END SECTION NOT TO SCALE



TR

IST

NO.	REVISION	BY	DATE
1	PER RIDEM COMMENTS	JPL	09/21/2021
	•	'	

JOB NO. 7247.00	DRAWN BY	J.P.L.
DWG. NO. 7247-00-Details.dwg	CHECKED	S.S.H.
SCALE:	APPROVED	S.S.H.

SHEET

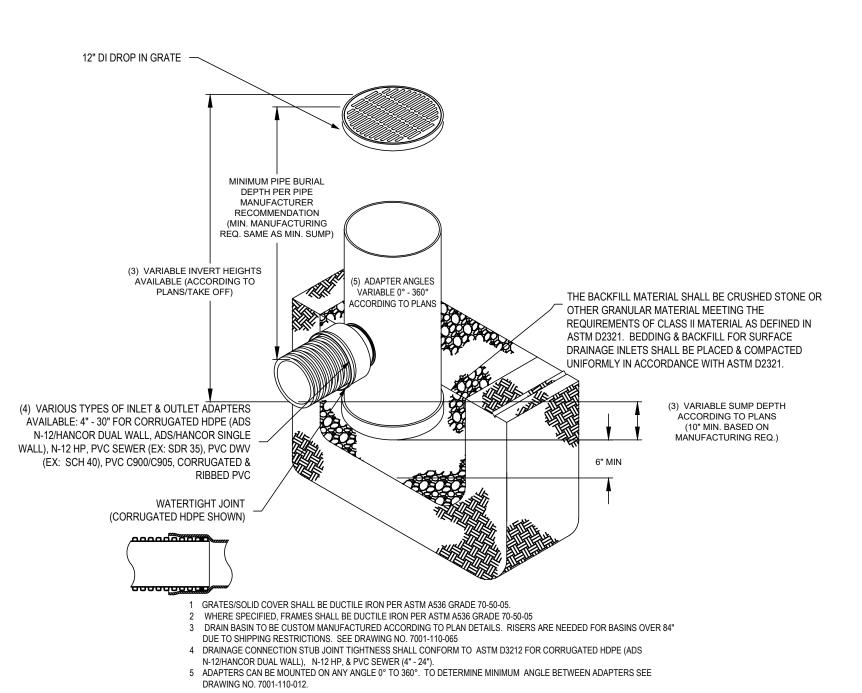
12 OF 13 SHEETS

DATE: AUGUST, 2021

-JUTE MESH TO BE PLACED ON BOTH SIDES OF RIP-RAP FOR EROSION STABILIZATION

4. USE ONLY SECTION A-A FOR ROOF DRAINS.

BOULDER WALL



NYLOPLAST 12" DRAIN BASIN
NOT TO SCALE

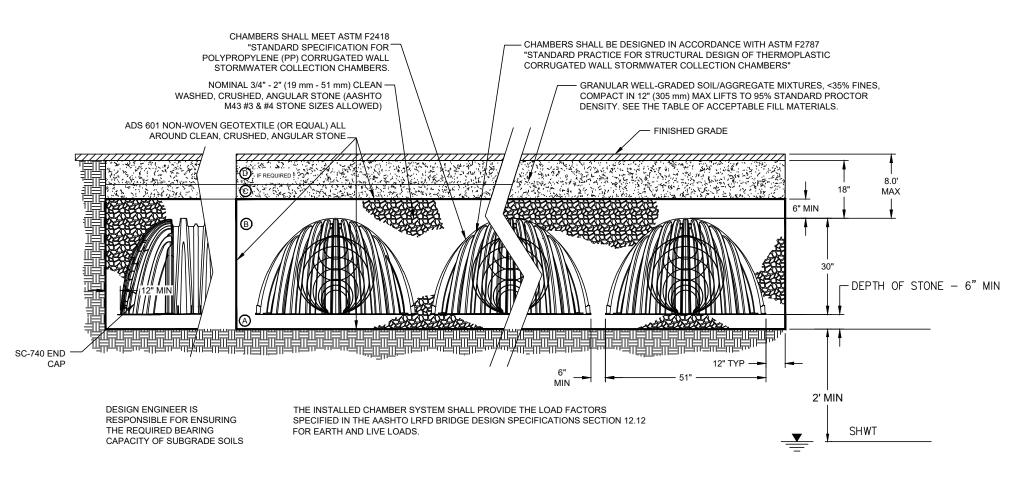
STORMWATER CHAMBER SPECIFICATIONS

- 1. CHAMBERS SHALL BE STORMTECH SC-740 OR APPROVED EQUAL.
- 2. CHAMBERS SHALL BE MADE FROM VIRGIN, IMPACT-MODIFIED POLYPROPYLENE COPOLYMERS.
- CHAMBER ROWS SHALL PROVIDE CONTINUOUS, UNOBSTRUCTED INTERNAL SPACE WITH NO INTERNAL SUPPORT PANELS THAT WOULD IMPEDE FLOW OR LIMIT ACCESS FOR INSPECTION.
 THE STRUCTURAL DESIGN OF THE CHAMBERS, THE STRUCTURAL BACKFILL AND THE
- INSTALLATION REQUIREMENTS SHALL ENSURE THAT THE LOAD FACTORS SPECIFIED IN THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, SECTION 12.12 ARE MET FOR: 1) LONG-DURATION DEAD LOADS AND 2) SHORT-DURATION LIVE LOADS, BASED ON THE AASHTO DESIGN TRUCK WITH CONSIDERATION FOR IMPACT AND MULTIPLE VEHICLE PRESENCES.
- 5. CHAMBERS SHALL CONFORM TO THE REQUIREMENTS OF ASTM F 2787, "STANDARD PRACTICE FOR STRUCTURAL DESIGN OF THERMOPLASTIC CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
- 7. ONLY CHAMBERS THAT ARE APPROVED BY THE ENGINEER WILL BE ALLOWED. THE CONTRACTOR SHALL SUBMIT (3 SETS) OF THE FOLLOWING TO THE ENGINEER FOR APPROVAL BEFORE DELIVERING CHAMBERS TO THE PROJECT SITE:
 - a. STRUCTURAL EVALUATION BY A REGISTERED STRUCTURAL ENGINEER THAT DEMONSTRATES THAT THE LOAD FACTORS SPECIFIED IN THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, SECTION 12.12 ARE MET.
 b. STRUCTURAL CROSS SECTION DETAIL ON WHICH THE STRUCTURAL CROSS
- 8. THE INSTALLATION OF CHAMBERS SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S LATEST INSTALLATION INSTRUCTIONS.

CHAMBER ELEVATION TABLE

SECTION IS BASED.

DWELLING UNI	NUMBER OF CHAMBERS	BOTTOM OF STONE	SHWT
1	2	27.00	25.00
2	2	27.00	25.00
3	2	24.00	22.00
4	2	24.00	22.00
5	2	27.00	25.00
6	2	27.00	25.00
7	2	26.00	23.00
8	2	26.00	23.00
9	2	27.00	25.00
10	2	27.00	25.00
11	2	27.00	25.00
12	2	27.00	25.00
13	2	26.50	24.50
14	2	26.50	24.50
15	2	27.00	25.00
16	2	27.00	25.00
17	2	26.50	22.50
18	2	26.50	22.50
19	2	26.50	24.50
20	2	26.50	24.50



CROSS SECTION

NOT TO SCALE

ACCEPTABLE FILL MATERIALS: STORMTECH SC-740 CHAMBER SYSTEMS

M	IATERIAL LOCATION	DESCRIPTION	AASHTO M43 DESIGNATION ¹	COMPACTION/DENSITY REQUIREMENT
(D)	FILL MATERIAL FOR LAYER 'D' STARTS FROM THE TOP OF THE 'C' LAYER TO THE BOTTOM OF FLEXIBLE PAVEMENT OR UNPAVED FINISHED GRADE ABOVE. NOTE THAT PAVEMENT SUBBASE MAY BE PART OF THE 'D' LAYER.	ANY SOIL/ROCK MATERIALS, NATIVE SOILS, OR PER ENGINEER'S PLANS. CHECK PLANS FOR PAVEMENT SUBGRADE REQUIREMENTS.	N/A	PREPARE PER ENGINEER'S PLANS. PAVED INSTALLATIONS MAY HAVE STRINGENT MATERIAL AND PREPARATION REQUIREMENTS.
©	FILL MATERIAL FOR LAYER 'C' STARTS FROM THE TOP OF THE EMBEDMENT STONE ('B' LAYER) TO 24" [610 mm] ABOVE THE TOP OF THE CHAMBER. NOTE THAT PAVEMENT SUBBASE MAY BE A PART OF THIS LAYER.	GRANULAR WELL-GRADED SOIL/AGGREGATE MIXTURES, < 35% FINES. MOST PAVEMENT SUBBASE MATERIALS CAN BE USED IN LIEU OF THIS LAYER.	3, 357, 4, 467, 5, 56, 57, 6, 67, 68, 7, 78, 8, 89, 9, 10	BEGIN COMPACTION AFTER 24" [610 mm] OF MATERIAL OVER THE CHAMBERS IS REACHED. COMPACT ADD- ITIONAL LAYERS IN 12" [305 mm] MAX LIFTS TO A MIN. 95% STANDARD PROCTOR DENSITY.
B	EMBEDMENT STONE SURROUNDING THE CHAMBERS FROM THE FOUNDATION STONE TO THE 'C' LAYER ABOVE.	CLEAN WASHED, CRUSHED, ANGULAR STONE, NOMINAL SIZE DISTRIBUTION BETWEEN 3/4 - 2 INCH [19 - 51 mm]	3, 4	NO COMPACTION REQUIRED.
A	FOUNDATION STONE BELOW CHAMBERS FROM THE SUBGRADE UP TO THE FOOT (BOTTOM) OF THE CHAMBER.	CLEAN WASHED, CRUSHED, ANGULAR STONE, NOMINAL SIZE DISTRIBUTION BETWEEN 3/4 - 2 INCH [19 - 51 mm]	3, 4	PLATE COMPACT OR ROLL TO ACHIEVE A 95% STANDARD PROCTOR DENSITY 2.

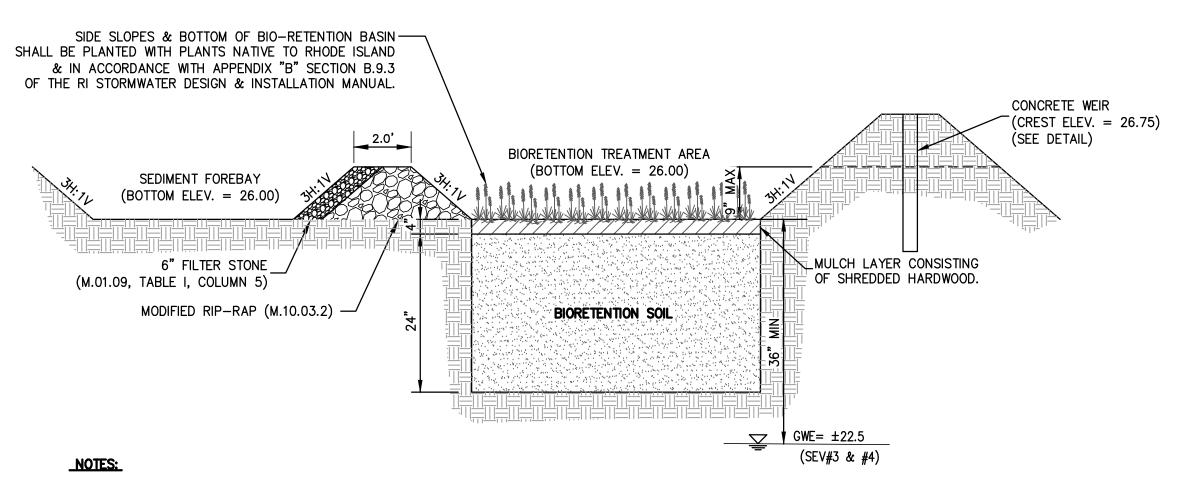
PLEASE NOTE:

1. THE LISTED AASHTO DESIGNATIONS ARE FOR GRADATIONS ONLY. THE STONE MUST ALSO BE CLEAN, CRUSHED, ANGULAR. FOR EXAMPLE, A SPECIFICATION FOR #4 STONE WOULD STATE: "CLEAN, CRUSHED, ANGULAR NO. 4 (AASHTO M43) STONE".

MATERIALS WHEN PLACED AND COMPACTED IN 9" [229 mm] (MAX) LIFTS USING TWO FULL COVERAGES WITH AN APPROPRIATE COMPACTOR.

2. AS AN ALTERNATE TO PROCTOR TESTING AND FIELD DENSITY MEASUREMENTS ON OPEN GRADED STONE, STORMTECH COMPACTION REQUIREMENTS ARE MET FOR 'A' LOCATION

BMP-2 SC-740 CHAMBERS



1. THE BIO-RETENTION FACILITY MUST NOT BE CONSTRUCTED UNTIL ALL CONTRIBUTING DRAINAGE AREAS HAVE BEEN STABILIZED.

3. THE BIO-RETENTION SOIL SHOULD BE A UNIFORM MIX, FREE OF STONES, STUMPS, ROOTS OR OTHER SIMILAR OBJECTS LARGER THAN ONE INCH. NO OTHER MATERIALS OR SUBSTANCES SHOULD BE MIXED OR DUMPED WITHIN THE BIO-RETENTION AREA THAT MAY BE HARMFUL TO PLANT GROWTH, OR PROVE A HINDRANCE TO THE PLANTING OR MAINTENANCE OPERATIONS. THE BIO-RETENTION SOIL SHOULD BE FREE OF NOXIOUS WEEDS. THE PLACEMENT OF THE SOIL SHALL BE PLACED 12 INCH LIFTS, LOOSELY COMPACTED (TAMPED LIGHTLY WITH A DOZER OR BACKHOE BUCKET). THE BIO-RETENTION SYSTEM SHALL UTILIZE SOIL HAVING THE FOLLOWING COMPOSITION:

SAND: 85-88%

SILT: 8-12% CLAY: 0-2% ORGANICS: 3-5% IN FORM OF LEAF COMPOST

4. USDA TEXTURAL ANALYSIS IS REQUIRED TO ENSURE THE BIO-RETENTION SOIL MEETS THE SPECIFICATION LISTED ABOVE. THE BIO-RETENTION AREA SOIL SHOULD ALSO BE TESTED FOR THE FOLLOWING CRITERIA:

pH RANGE 5.2—7.0

MAGNESIUM NOT TO EXCEED 32 ppm
PHOSPHOROUS NOT TO EXCEED 69 ppm
POTASSIUM NOT TO EXCEED 78 ppm
SOLUBLE SALTS NOT TO EXCEED 500 ppm

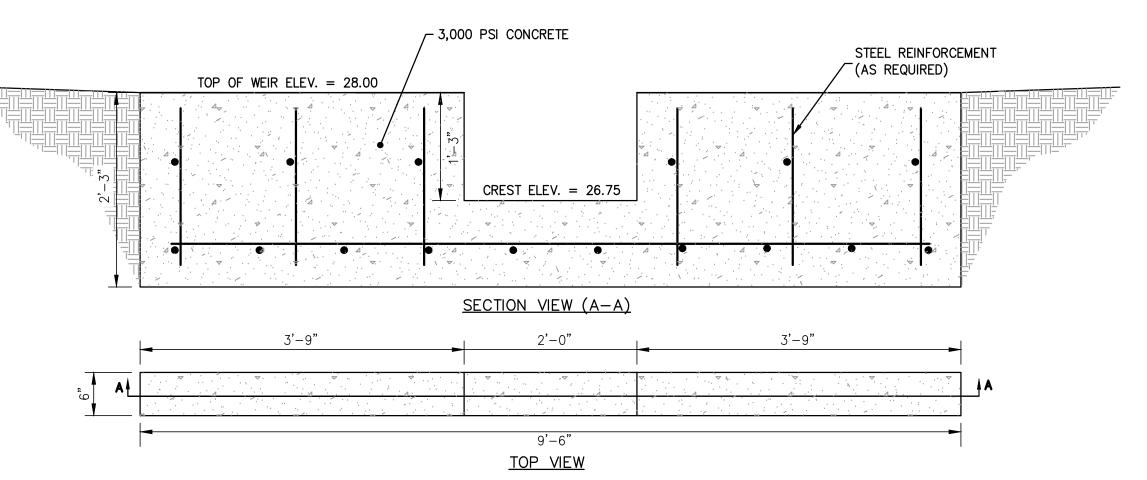
5. THE CONTRACTOR SHALL WAIT 3 DAYS AND CHECK FOR SETTLING BEFORE INSTALLING PLANTINGS.

6. MULCH LAYER MUST BE A FINELY SHREDDED, WELL-AGED (6-12 MONTHS) ORGANIC HARDWOOD. BARK DUST MULCHES AND WOOD CHIPS WILL FLOAT AND MOVE TO THE PERIMETER OF THE BIO-RETENTION AREA DURING A STORM EVENT AND ARE NOT ACCEPTABLE. CONTRACTOR MUST SUBMIT A SAMPLE OF THE MULCH.

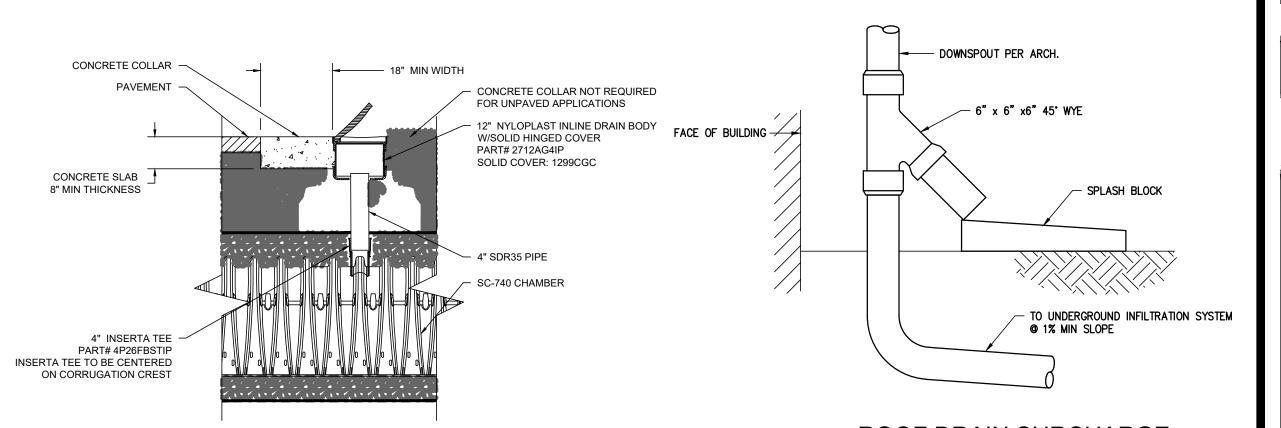
7. CONTRACTOR MUST REFER TO LANDSCAPE DRAWINGS FOR DETAILS AND FREQUENCY OF WATER BIO-RETENTION PLANTINGS.

BMP-1 BIO-RETENTION BASIN

NOT TO SCALE



CONCRETE WEIR



SC-740 INSPECTION PORT DETAIL

ROOF DRAIN SURCHARGE
WITH SPLASH BLOCK
NOT TO SCALE

CONSTRUCTION DETAILS - 5

FOR
WATERSIDE AT LITTLE POND
(A.P. 350, LOT 583)
SITUATED ON
SANDY LANE
SANDY LANE
WARWICK, RHODE ISLAND
PREPARED FOR

NO.	REVISION	BY	DATE
1	PER RIDEM COMMENTS	JPL	09/21/2021

S, INC.
INEERSISURVEYORS
IMENTAL SCIENTISTS
Garofalo & Associates ©
These drawings are the property of the engineer/surveyor and have been prepared for the owner, for this project at this site and are not to be used for any other purpose, location or owner without written consent of this owner or one of its directors.

85 CORLISS STREET
P.O. BOX 6145
PROVIDENCE, R.I. 02940
TEL. 401-273-6000

JOB NO.
7247.00
DWG. NO.
7247-00-Details.dwg

SCALE:

AS SHOWN

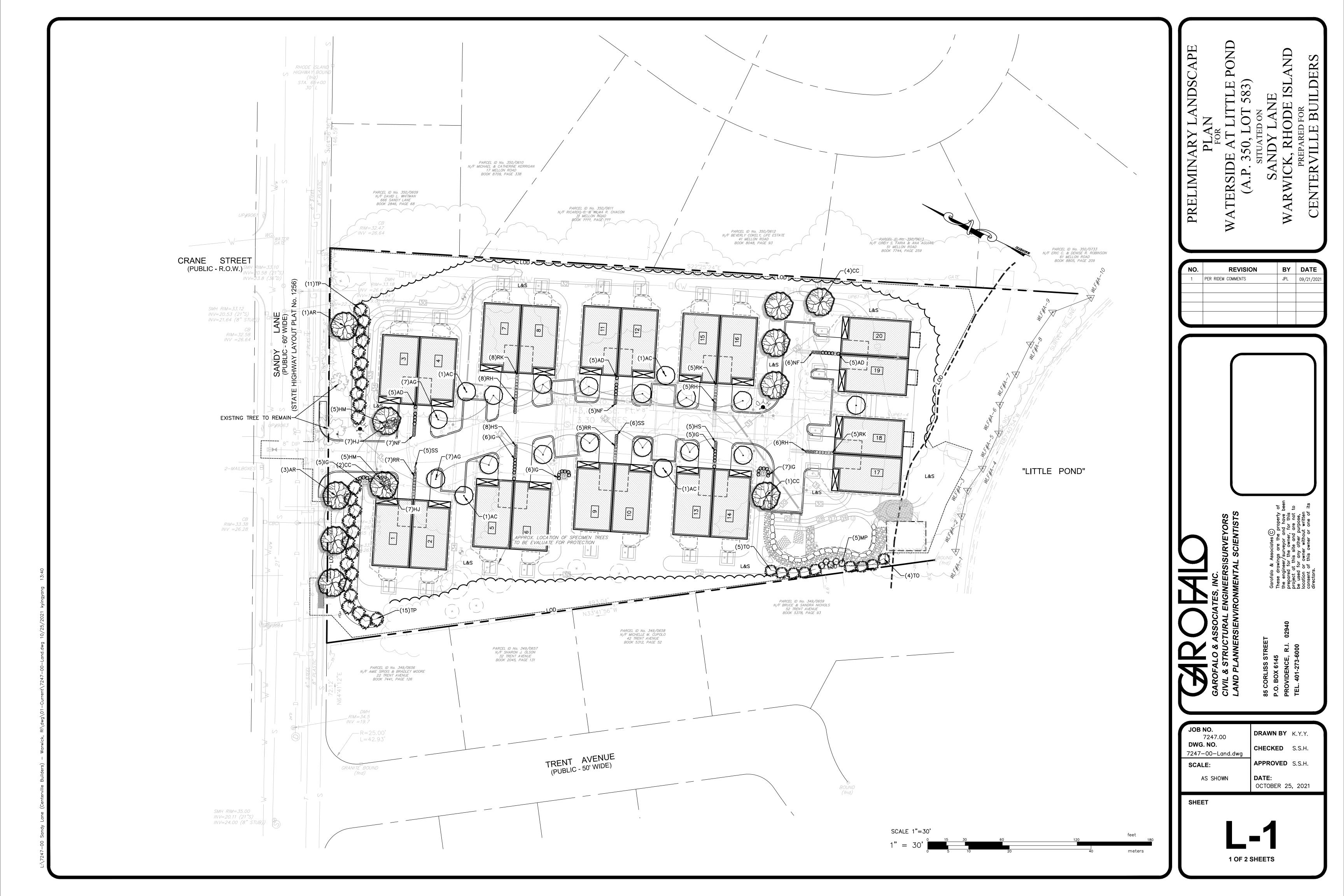
DRAWN BY
J..P.L.
CHECKED
S.S.H.

APPROVED
S.S.H.

DATE:
AUGUST, 2021

SHEET

C-12



GENERAL LANDSCAPE NOTES:

- 1. THE CONTRACTOR SHALL INSTALL ALL PLANT MATERIAL IN CONFORMANCE WITH THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. LATEST EDITION.
- 2. ALL PLANT MATERIAL SHALL MEET THE AMERICAN OF NURSERYMEN STANDARDS FOR NURSERY STOCK, LATEST EDITION, AND ITS AMENDMENTS. PLANT ONLY DURING SEASONS NORMAL TO THE PARTICULAR VARIETY. NO SUBSTITUTIONS SHALL BE ACCEPTED WITHOUT PRIOR APPROVAL BY THE LANDSCAPE ARCHITECT.
- 3. ALL PLANT MATERIAL FURNISHED BY THE CONTRACTOR SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR. IF THE FIRST YEAR ENDS DURING THE DORMANT PERIOD, THE GUARANTEE SHALL EXTEND TO THE START OF THE NEXT LEAFING. DURING THE ONE-YEAR GUARANTEE PERIOD THE CONTRACTOR SHALL EMPLOY ACCEPTED HORTICULTURAL PRACTICES TO KEEP THE PLANTS INSTALLED IN LIVING, HEALTHY CONDITION. FOLLOWING THE ONE-YEAR GUARANTEE PERIOD THE OWNER/OWNERS SHALL INSPECT ALL PLANT MATERIAL FOR HEALTHY ORNAMENTAL QUALITY. AT THAT TIME SHOULD ANY PLANT MATERIAL BE FOUND IN INADEQUATE HEALTH THE CONTRACTOR SHALL BE RESPONSIBLE TO REPLACE THE PLANT IN KIND AND PROVIDE A ONE-YEAR GUARANTEE FOR THE REPLACEMENT PLANT
- 4. INSTALL ALL PLANT MATERIAL AS SHOWN IN DETAILS.
- 5. STAKE ALL TREES OVER 5' AS SHOWN ON DETAILS. REMOVE STAKES AT THE END OF THE GUARANTEE
- 6. THE CONTRACTOR IS RESPONSIBLE FOR KEEPING THE SITE CLEAN OF MISCELLANEOUS DEBRIS THROUGHOUT THE CONSTRUCTION PERIOD. ALL WASTE MATERIAL IS TO BE DISPOSED OF IMMEDIATELY TO AN OFFSET LOCATION, UNLESS OTHERWISE INDICATED ON THE PLANS.
- 7. THE CONTRACTOR SHALL PERFORM ALL WORK IN ACCORDANCE WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS, AND SHALL OBTAIN ALL NECESSARY PERMITS FOR THE PROJECT.
- 8. LAYOUT: ALL NOTES AND DIMENSIONS ARE TYPICAL UNLESS OTHERWISE NOTED. ALL DIMENSIONS ARE SQUARE (PARALLEL OR PERPENDICULAR) UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL NOTIFY THE OWNER/OWNER'S REPRESENTATIVE IMMEDIATELY IN THE EVENT OF ANY DISCREPANCIES FOUND IN THE CONTRACT DOCUMENTS AND/OR IN THE FIELD, OR ON CONDITIONS UNCOVERED IN THE WORK WHICH ARE NOT REFLECTED IN THE PLANS.
- 9. PROTECTION OF EXISTING PLANTINGS: MAXIMUM EFFORT SHOULD BE MADE TO SAVE TREES OF OTHER PLANT SPECIMENS WHICH ARE LARGE FOR THEIR SPECIES, RARE TO THE AREA, OR OF SPECIAL HORTICULTURAL OR LANDSCAPE VALUE. CONTACT OWNER/LANDSCAPE ARCHITECT BEFORE REMOVING ANY SPECIMEN OF THIS TYPE UNLESS OTHERWISE NOTED ON THE PLANS. NO MATERIAL OR TEMPORARY SOIL DEPOSITS SHALL BE PLACED WITHIN THE DRIP LINE OF SHRUBS OR TREES DESIGNATED ON THE LANDSCAPE PLAN TO BE RETAINED. PROTECTIVE BARRIERS ARE TO BE INSTALLED AROUND EACH PLANT AND/OR GROUP OF PLANTS THAT ARE TO REMAIN ON THE SITE. BARRIERS SHALL NOT BE SUPPORTED BY THE PLANTS THEY ARE PROTECTING, BUT SHALL BE SELF SUPPORTING. THEY SHALL BE OF MINIMUM OF FOUR FEET (4') HIGH AND CONSTRUCTED OF A DURABLE MATERIAL, SUCH AS SNOW OR SILT FENCE, THAT WILL LAST UNTIL CONSTRUCTION IS COMPLETED.
- 10. PRUNING: THE CONTRACTOR SHALL CAREFULLY PRUNE BRANCHES IN THE WAY OF CONSTRUCTION BY USING ONLY APPROVED METHODS AND TOOLS. THE USE OF AXES FOR TRIMMING OR SPURS FOR CLIMBING WILL NOT BE PERMITTED.
- 11. UTILITIES: THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR ANY AND ALL UTILITY DAMAGE INCLUDING DAMAGE THAT MAY OCCUR TO NEW UTILITIES. THE CONTRACTOR IS REQUIRED TO CONTACT DIG-SAFE PRIOR TO COMMENCING ANY SITE CONSTRUCTION. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- 12. DISTURBED AREAS: ALL AREAS DISTURBED DURING THE COURSE OF CONSTRUCTION ARE TO BE RESTORED TO ORIGINAL (OR BETTER) CONDITION BY THE CONTRACTOR BEFORE COMPLETION OF THE PROJECT, AND ARE SUBJECT TO APPROVAL BY THE LANDSCAPE ARCHITECT AND OWNER. ALL GRASS AREAS DISTURBED DURING CONSTRUCTION SHALL BE FINE RAKED TO REMOVE STONES AND LOAMED AND SEEDED AS PER SPECIFICATIONS.
- 13. DRAINAGE SYSTEMS: CONTRACTOR IS RESPONSIBLE FOR GENERAL CLEAN—OUT OF ALL CATCH BASINS, MANHOLES, AND/OR OTHER DRAINAGE FEATURES ON THE SITE WHICH HAVE ACCUMULATED SEDIMENT AS A RESULT OF CONSTRUCTION ACTIVITIES.

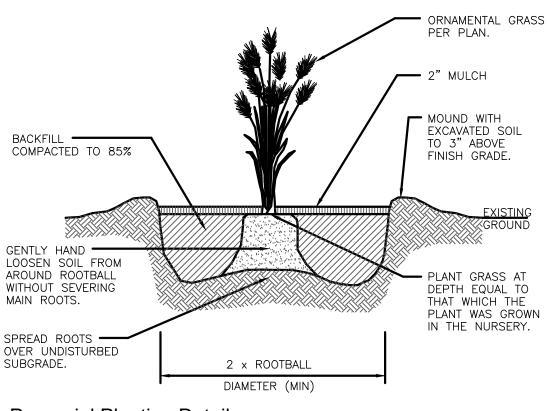
LOAM AND SEEDING

APPLICATION OF LOAM AND SEED SHALL BE IN ACCORDANCE WITH APPLICABLE SECTIONS OF THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, LATEST EDITION.

- 1. LOAM MATERIAL TO BE FURNISHED SHALL CONSIST OF SCREENED LOOSE, FRIABLE, FINE SANDY LOAM OR SANDY LOAM, AS DEFINED BY THE USDA'S SOIL CONSERVATION SERVICE IN THE SOIL SURVEY MANUAL ISSUED IN 1993, FREE OF SUBSOIL, REFUSE, STUMPS, ROOTS, ROCKS, COBBLES, STONES, BRUSH, NOXIOUS WEEDS, LITTER AND OTHER MATERIALS WHICH ARE LARGER THAN 1/2-INCH IN ANY DIMENSION AND WHICH WILL PREVENT THE FORMATION OF A SUITABLE SEED BED. ORGANIC MATTER SHALL CONSTITUTE NOT LESS THAN 5 PERCENT NOR MORE THAN 20 PERCENT OF THE LOAM AS DETERMINED BY LOSS-ON-IGNITION OF OVEN DRIED SAMPLES THAT HAVE BEEN DRAWN BY THE ENGINEER, UNLESS
- OTHERWISE SPECIFIED OR DIRECTED. THE LOAM SHALL HAVE AN ACIDITY RANGE OF 5.5 PH TO 7.6 PH. 2. LOAM MOVED DURING THE COURSE OF CONSTRUCTION SHALL BE RETAINED AND DISTRIBUTED WITHIN THE SITE IN ACCORDANCE WITH THE LANDSCAPE PLAN. STOCKPILED LOAM SHALL NOT BE MIXED WITH ANY

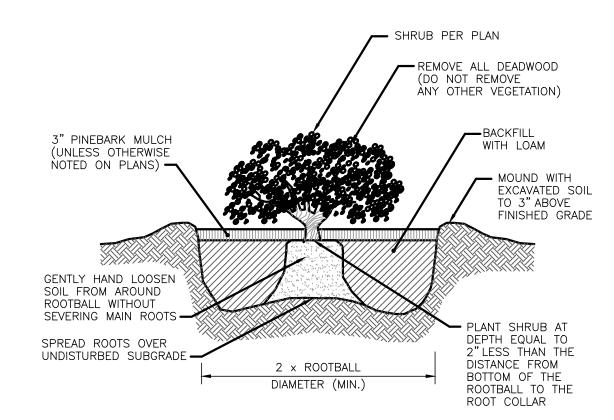
SUBSOIL OR UNSUITABLE MATERIAL. ALL EXISTING LOAM REMAINS THE PROPERTY OF THE OWNER.

- 3. LOAM SHALL BE SPREAD TO A DEPTH OF 4" IN SEEDING AREAS AND PLACED TRUE TO LINES AND GRADES SHOWN ON THE PLANS.
- 4. THE LOAMED SURFACE SHALL BE GRADED, AND ALL ROOTS, SODS, WEEDS, COBBLES OR STONES WITH ANY DIMENSION GREATER THAN 1 INCH SHALL BE REMOVED AND LEGALLY DISPOSED OF. AFTER SHAPING AND GRADING, ALL TRUCKS AND OTHER EQUIPMENT NOT REQUIRED TO PERFORM SEEDING, MULCHING OR MOWING OPERATIONS SHALL BE EXCLUDED FROM THE LOAMED AREAS.
- 5. LIME (GROUND OR PELLETIZED) SHALL BE APPLIED DRY AND SPREAD EVENLY OVER THE ENTIRE SURFACE TO BE SEEDED. UNLESS OTHERWISE SPECIFIED, THE APPLICATION RATE SHALL BE 1 TON PER ACRE. RAKING SHALL BE COMPLETED AFTER THE FERTILIZER HAS BEEN APPLIED.
- 6. ALL SEED DELIVERED TO THE JOB SHALL BE IN CONTAINERS LABELED IN ACCORDANCE WITH PROVISIONS OF THE RHODE ISLAND SEED ACT OF 1956 (VOLUME 8, TITLE 2, CHAPTER 6) AND ITS AMENDMENTS AS PROVIDED FOR AGRICULTURAL SEED OFFERED FOR SALE. ONLY THE CURRENT YEARS SEED SHALL BE ACCEPTED.
- 7. SEED SHALL BE APPLIED ONLY DURING THE FOLLOWING DATES: SPRING SEEDING: MARCH 15 TO MAY 31. FALL SEEDING: AUGUST 15 TO OCTOBER 15.
- 6. THE CONTRACTOR SHALL WATER ALL SEEDED AREAS AND RESEED AS NECESSARY FOR A PERIOD OF 2-MONTHS OR UNTIL A HEALTH STAND OF GRASS IS ESTABLISHED AND APPROVAL HAS BEEN MADE BY THE OWNER/LANDSCAPE ARCHITECT.

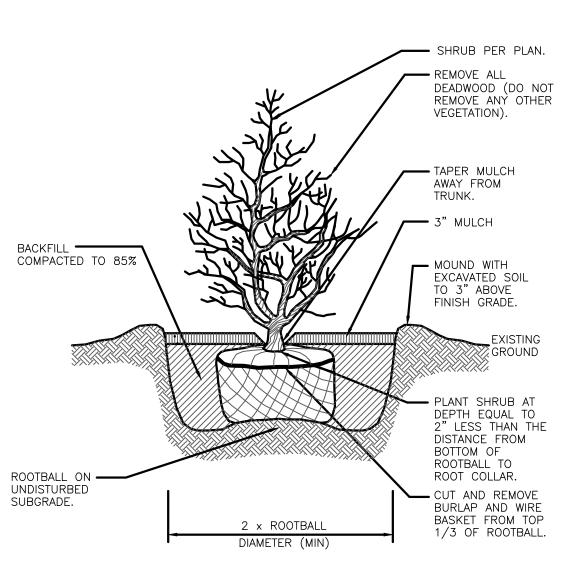


Perennial Planting Detail NOT TO SCALE - DO NOT CUT LEADER TREE PER PLAN. REMOVE ALL DEADWOOD (DO NOT REMOVE ANY OTHER VEGETATION). - GUY WEBBING ATTACHED NO HIGHER THAN 1/2 AND NO LOWER THAN 1/3 THE HEIGHT OF THE REMOVE ALL NURSERY TAPER MULCH PROTECTION AWAY FROM DEVICES PRIOR TRUNK. TO PLANTING. **7** 3" MULCH BACKFILL -MOUND WITH COMPACTED TO 85% EXCAVATED SOIL 3" ABOVE FINISH GRADE. GROUND 2−2"x2"x varies **-**PLANT TREE AT HARDWOOD STAKES. DEPTH EQUAL TO DRIVE 3' INTO LESS THAN THE GROUND OUTSIDE DISTANCE FROM OF ROOTBALL. BOTTOM OF ROOT COLLAR. ROOTBALL ON CUT AND REMOVE UNDISTURBED





Container Grown Shrub and Perennial Planting Detail NOT TO SCALE



REMOVE ALL

PROTECTION

TO PLANTING.

DEVICES PRIOR

COMPACTED TO 85%

2-2"x2"x varies **─**

HARDWOOD STAKES.

DRIVE 3' INTO

OF ROOTBALL.

GROUND OUTSIDE

ROOTBALL ON -

Evergreen Tree Planting Detail

UNDISTURBED

SUBGRADE.

NURSERY

B & B Shrub Planting Detail

BURLAP AND WIRE

BASKET FROM TOP

1/3 OF ROOTBALL.

PLANT LIST:

SUBGRADE.

CANOPY TREES					
KEY	BOTANICAL NAME COMMON NAME	QTY.	SIZE	NOTE	
AR	ACER RUBRUM 'FRANKSRED' RED SUNSET SWAMP MAPLE	4	2.5"-3" CAL		
CC	CARPINUS CAROLINIANA AMERICAN HORNBEAM	7	2.5"-3" CAL		

FLOW	ERING TREES			
KEY	BOTANICAL NAME COMMON NAME	QTY.	SIZE	NOTE
AC	AMELANCHIER CANADENSIS SERVICEBERRY	4	7'-8' CLUMP	
AG	ACER GRISEUM PAPERBARK MAPLE	14	7'-8' CLUMP	

	EVER	GREEN TREES			
	KEY	BOTANICAL NAME COMMON NAME	QTY.	SIZE	NOTE
	ТО	THUJA OCCIDENTALIS AMERICAN ARBORVITAE	9	5'-8' B¢B	
	TP	THUJA PLICATA 'GREEN GIANT' GREEN GIANT ARBORVITAE	26	5'-8' B¢B	
Į		GREEN GIANT ARBORVITAE		D#D	

GENERAL SEEDING MIX (L&S)

	PERCENT BY WEIGHT	PERCENT BY VOLUME
CHEWINGS FESCUE IMPROVED VARIETIES	30	85
KENTUCKY BLUEGRASS IMPROVED VARIETIES	30	90
PERENNIAL RYEGRASS IMPROVED VARIETIES	40	90
APPLICATION RATE: 200	LBS / ACRE	

EVERGREEN/ DECIUOUS SHRUBS					
KEY	BOTANICAL NAME COMMON NAME	QTY.	SIZE	NOTE	
AD	AZALEA 'DELAWARE VALLY WHITE' WHITE AZALEA	15	# 3 CONTIANER		
НМ	HYDRANGEA MAC. 'ENDLESS SUMMER' ENDLESS SUMMER HYDRANGEA	10	#3 CONTAINER		
IG	ILEX GLABRA 'COMPACTA' COMPACT INKBERRY	29	# 3 CONTAINER		
MP	MYRICA PENNSYLVANICA BAYBERRY	5	#3 CONTAINER		
RK	ROSA 'RADCON' KNOCK OUT PINK PINK KNOCK OUT ROSE	18	#3 CONTAINER		
RR	ROSA 'RADSUNNY' SUNNY KNOCK OUT ROSE	12	#3 CONTAINER		

PERENNIALS/GROUNDCOVERS/ VINES				
KEY	BOTANICAL NAME COMMON NAME	QTY.	SIZE	NOTE
HS	HEMEROCALLIS 'STELLA DE ORO STELLA DE ORO DAYLILY	13	#2 CONTAINER	
NF	NEPETA FAASSENII 'WALKER'S LOW' WALKER'S LOW CATMINT	18	#2 CONTAINER	
RH	RUDBECKIA HIRTA BLACK EYED SUSAN	19	#2 CONTAINER	
55	SCHIZACHYRIUM SCOPARIUM LITTLE BLUESTEM	11	#2 CONTAINER	

S

DO NOT CUT

REMOVE ALL

VEGETATION).

GUY WERRING

ATTACHED NO

HIGHER THAN 1/2

AND NO LOWER

THAN 1/3 THE

HEIGHT OF THE

AWAY FROM

- MOUND WITH

EXCAVATED SOIL

50 3" AROVE

FINISH GRADE.

DEPTH EQUAL TO

DISTANCE FROM

BOTTOM OF

ROOTBALL TO

ROOT COLLAR

2 x ROOTBALL

CUT AND REMOVE

BURLAP AND WIRE

BASKET FROM TOP

1/3 OF ROOTBALL.

LESS THAN THE

GROUND

TREE PER PLAN.

DEADWOOD (DO NOT

REMOVE ANY OTHER

LEADER

NO.	REVISION	BY	DATE
1	PER RIDEM COMMENTS	JPL	09/21/2021
	1		

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JOB NO. DRAWN BY K.Y.Y. 7247.00 DWG. NO. **CHECKED** S.S.H. 7247-00-Land.dwg **APPROVED** S.S.H. SCALE:

SHEET

AS SHOWN

2 OF 2 SHEETS

OCTOBER 25, 2021

